

From: VTA Board Secretary
Sent: Monday, July 8, 2024 12:23 PM
To: VTA Board of Directors
Cc: VTA Board Secretary
Subject: VTA Information: Ridership for May 2024

VTA Board of Directors:

Attached is a memorandum from Derik Calhoun, Chief Operating Officer, regarding VTA ridership for May 2024.

If you have any questions, please reply to this e-mail.

Thank you.

Office of the Board Secretary
Santa Clara Valley Transportation Authority
3331 N. First Street
San Jose, CA 95134
408.321.5680
board.secretary@vta.org



Date: June 26, 2024
Current Meeting: NA
Board Meeting: NA

BOARD MEMORANDUM

TO: Santa Clara Valley Transportation Authority
Board of Directors

THROUGH: Carolyn Gonot, General Manager/CEO

FROM: Derik Calhoun, Chief Operating Officer

SUBJECT: VTA Ridership – May 2024

Policy-Related Action: NA

Government Code Section 84308 Applies: No

INFORMATION ITEM

EXECUTIVE SUMMARY:

To present the bus and light rail ridership performance of the Santa Clara Valley Transportation Authority.

STRATEGIC PLAN/GOALS:

The report is an information item only.

BACKGROUND:

This report is routinely produced after each month. This report is for the month of May 2024.

CLIMATE IMPACT:

The report is an information item and will have no impact on climate change.

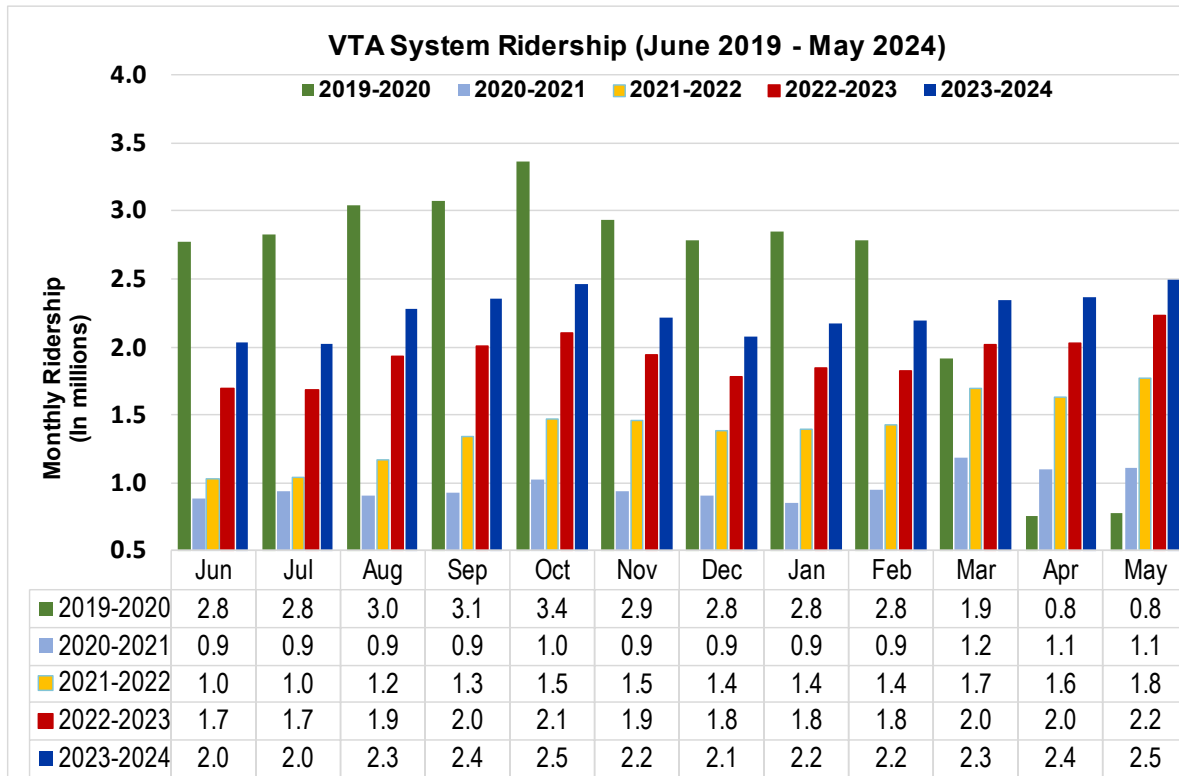
DISCUSSION:

May 2024 total monthly system ridership was 2,493,032, an increase of 12% over May 2023. The total monthly bus ridership was 2,075,347, an increase of 12.3% over May 2023. The total monthly light rail ridership in May 2024 was 417,685, an increase of 10.6% over May 2023. May 2024 monthly system ridership increased by 5.2% compared to April 2024. The calendar year-to date through May 2024 system ridership (bus and light rail) is at about 79.8% of a pre-pandemic benchmark.

Levi's Events: There were three events at Levi's stadium in May 2024. There was a San Jose Earthquakes vs. Los Angeles Football Club game and two Luke Combs concerts. The average ridership from the events was 6,896 riders.

Ridership	May-24	Compared to:				CYTD				
		May-23	Percent change	Apr-24	Percent change	Current 2024	Prior 2023	Percent change	Pre-Pandemic/NTSP	Percent change
						Jan-May	Jan-May		Jan-May	
Bus	2,075,347	1,848,641	12.3%	1,965,191	5.6%	9,560,861	8,183,206	16.8%	11,094,411	-13.8%
Light Rail	417,685	377,730	10.6%	403,686	3.5%	1,998,636	1,755,997	13.8%	3,393,434	-39.3%
System	2,493,032	2,226,371	12.0%	2,368,877	5.2%	11,559,497	9,939,203	16.3%	14,487,845	-20.2%

The system ridership (Bus and light rail) in millions from June 2019 through May 2024 is presented in the chart below:



Note: Pre-pandemic data reflected in the chart for the months of June 2019 through February 2020 and a few days in March 2020. Mid-March 2020 onwards had impacts from COVID-19.

The boardings per total hour and boardings per revenue hour for bus and rail for May 2024 are shown in the table below:

	Boardings per Total hour ¹			Boardings per Revenue hour ²		
	May 2024	May 2023	Percent Change	May 2024	May 2023	Percent Change
Bus	16.5	15.7	4.8%	17.6	16.7	5.6%
Light Rail	30.7	29.6	3.7%	33.0	31.4	5.1%

Total hours – Includes revenue hours and deadhead hours (hours that a vehicle travels when out of revenue service). ² Revenue hours - Scheduled hours of service available to passengers for transport on the routes. Includes recovery/layover time.

From: VTA Board Secretary
Sent: Friday, July 12, 2024 3:42 PM
To: VTA Board of Directors
Cc: Gonot, Carolyn; Maguire, Tom
Subject: From VTA: BSVII Station Design Update for VTA Board Members
Importance: High

VTA Board of Directors,

Attached is an update and status report on the May 2, 2024, VTA Board Referral regarding station design for VTA's BART Silicon Valley Phase II Extension Project.

The Board also requested a copy of the Federal Transit Administration's (FTA) Project Management Oversight Contractor (PMOC Report). The monthly PMOC reports are available on the VTA website. Please click this link to access:

<https://www.vta.org/projects/bart-sv/phase-ii#accordion-federal-funding>. Scroll to the Funding Strategy section of the page and click on the "federal funding" tab to view the monthly reports.

For questions, please reply to this email.

Thank you.

Santa Clara Valley Transportation Authority
3331 North First Street,
San Jose, CA 95134-1927



MEMORANDUM

TO: Santa Clara Valley Transportation Authority
Board of Directors

THROUGH: General Manager/CEO, Carolyn M. Gonot

FROM: Chief Capital Megaprojects Delivery Officer, Tom Maguire

DATE: July 12, 2024

SUBJECT: VTA's BART Silicon Valley Phase II Extension Project – Update on May 2024 Board Referral on Station Design

SUMMARY:

This memo provides an update and status report on the May 2, 2024, VTA Board Referral regarding station design for VTA's BART Silicon Valley Phase II Extension Project.

BACKGROUND:

At the May 2024 VTA Board of Directors Meeting, the Board issued a referral providing specific direction for VTA staff to address stakeholders and community concerns about the current design of the 28th Street/Little Portugal BART Station (28th Street/Little Portugal) and Diridon BART Station (Diridon), and the desire for a south entrance at the Downtown San José BART Station (Downtown San José).

DISCUSSION:

In response to this Board Referral, VTA drafted the attached report (Attachment A), Referral Report Revision No. 0 (RR No. 0). As requested by the Board, this report provides a comprehensive response plan, summarizing the stakeholder and community concerns received, outlines VTA's approach to address these concerns and feedback, describes the station options and evaluation process to be considered, and approach to develop a future staff recommendation.

VTA staff intends to submit a second written report, Referral Report Revision No. 1 (RR No. 1), at the September 5th Board of Directors meeting. RR No. 1 will include VTA's evaluation findings and recommendations of options to advance. RR No. 1 will describe the refined station

options, evaluation findings, and staff recommended station options with a financial strategy, work plan, and recommended approach and timeline for the recommended options.

Stakeholder engagement with community members, City of San Jose, BART, Community Working Groups (CWG), Design Review Committees (DRC), Diridon Integrated Station Concept (DISC) Technical Directors, and other stakeholders will continue for each station. As requested by the Board Referral, the CWG structure will be modified to include hybrid meetings (both virtual and in-person) with staff and consultant support, and CWG review of project documents prior to submission of RR No. 1.

Additionally, staff prepared the attached Station Design and Engagement Update document (Attachment B) for Congresswoman Zoe Lofgren to provide her a summary on how VTA is addressing the feedback received from stakeholders and the Board Referral, specifically in regard to the request for a second entrance at the Downtown San Jose BART Station.

Attachment A – VTA’s BART Silicon Valley Phase II Extension Project Referral Report Revision No. 0, June 26, 2024

Attachment B – Station Design and Engagement Update

VTA's BART SILICON VALLEY

PHASE II EXTENSION PROJECT

REFERRAL REPORT

Revision No. 0,
Revision June 26, 2024
(Rev No. 0 & June 26, 2024)

**BART Silicon Valley Phase II
Extension Project**



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REFERRAL REPORT

Revision History

REV	DATE	DESCRIPTION
0	6/26/2024	VTA's approach for addressing the requests and directions from VTA Board of Directors in the Referral dated May 2, 2024.

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Appendices

APPENDIX A: VTA BOARD REFERRAL (ISSUED MAY 2, 2024)

Acronyms and Abbreviations

ABBREVIATION	DEFINITION
ACE	Altamont Corridor Express
APC	Airport Connector
BART	San Francisco Bay Area Rapid Transit District
BOD	Board of Directors
BOH	BART's Back-of-House facilities
BSV	BART Silicon Valley
BSVII	BART Silicon Valley Phase II Extension Project
CEQA	California Environmental Quality Act
CFD	Community Facilities District
CHSRA	California High Speed Rail Authority
CP1	Contract Package 1 – Systems
CP2	Contract Package 2 – Tunnel & Trackwork
CP3	Contract Package 3 – Newhall Yard & Santa Clara Station
CP4	Contract Package 4 – Underground Stations
CSJ	City of San José
CWG	Community Working Group
DBC	Diridon Business Case
DDF	Design Development Framework
DISC	Diridon Integrated Station Concept
DOT	City of San José Department of Transportation
DPW	City of San José Department of Public Works
DRC	Design Review Committee
DTSJ	Downtown San José
DTW	Downtown West
EVA	Emergency Ventilation Access
FFGA	Full Funding Grant Agreement
FTA	Federal Transit Administration
GEC	General Engineering Contractor
JPAB	Diridon Joint Advisory Policy Board
KST	Kiewit Shea Traylor
LRT	Light Rail Transit

ABBREVIATION	DEFINITION
MTC	Metropolitan Transportation Commission
NEPA	National Environmental Policy Act
O&M	Operations and Maintenance
OED	City of San José Office of Economic Development
PBCE	City of San José Planning & Building Code Enforcement
PCJPB	Peninsula Corridor Joint Powers Board (Caltrain)
RE	Real Estate (VTA Department)
ROW	Right-of-way
SJSU	San José State University
SEM	Sequential Excavation Method
SIF	Station Infrastructure Facilities
SME	Subject Matter Expert
SOE	Support of Excavation
SPUR	San Francisco Bay Area Planning and Urban Research Association
TD	Technical Director
TIRCP	Transit and Intercity Rail Capital Program
TOC	Transit-Oriented Communities
TOD	Transit-Oriented Development
TPSS	Traction Power Substation
VTA	Santa Clara Valley Transportation Authority

Definitions

TERM	DEFINITION
Center platform configuration	A station configuration in which a single platform is in the center between two tracks.
Design Development Framework	A framework of design standards and principles that guides VTA, future development partners and stakeholders in developing transit-oriented development infrastructure.
Emergency egress	Escape routes from tunnels in the event of emergencies.
Fan plants	Devices that channel air in and out of underground tunnels.
Full funding grant agreement	A type of grant agreement used by the Federal Transit Administration for investments in new fixed guideway systems. In exchange for \$25 million or more in FTA funding, grant recipients agree to complete their new starts projects on time, within budget, in accordance with all federal regulations, and to cover all cost increases after the awarding and execution of the agreement.
Knock-out panel	A design feature for one or more of the tunnel lining segments that accommodates the possibility of removing a panel to facilitate future connection with minimal impact to the structural integrity of the tunnel.
Stacked platform configuration	A station configuration in which one platform provides access per track across multiple vertical levels.
Station entrance building	Building(s) at a rail station that do(es) not house the tracks and platforms. The station entrance is usually located above ground and provides service to passengers. Station entrance building is also referred to as BART's Headhouse.
Station infrastructure facilities	Facilities located out of view from the public that are used to support the operation of rail infrastructure and services. Station infrastructure facilities is also referred to as BART's Back-of-House facilities.
Switchback	A configuration in which escalators ascend or descend to a landing, turn 180 degrees, and ascend or descend in the opposite direction.

1 EXECUTIVE SUMMARY

VTA's BART Silicon Valley Phase II Extension Project (VTA's BSVII) is a six-mile, four station extension of Bay Area Rapid Transit (BART) service from the Berryessa/North San José Station through downtown San José to the City of Santa Clara. VTA Board of Directors (BOD) on May 2, 2024 issued a Referral with specific direction for VTA staff to address stakeholders and community concerns about the current design of the 28th Street/Little Portugal BART Station (28th Street/Little Portugal) and Diridon BART Station (Diridon), and the desire for a south entrance at the Downtown San José BART Station (Downtown San José) (**Appendix A**).

VTA staff will provide two written reports (Referral Report Revision No. 0 in July 2024 and Referral Report Revision No. 1 in September 2024) to the VTA BOD and BSVII Oversight Committee.

- Report Revision No. 0 (RR No. 0) represents VTA staff's response plan. RR No. 0 summarizes stakeholder and community concerns received, outlines VTA's approach to address these concerns and feedback, describes the station options and evaluation process, and approach to develop a staff recommendation.
- Report Revision No. 1 (RR No. 1) represents VTA staff's evaluation findings and recommendations of options to advance. RR No. 1 will build upon RR No. 0 by describing the refined station options, evaluation findings, and staff recommended station options with a financial strategy, work plan, and recommended approach and timeline for the recommended options.

RR No. 0 contains the following sections:

Section 2 provides background information, details about the Board Referral, and an overview of the referral response process. The referral response process will include engagement with stakeholders and community to gather feedback and evaluate station options based on the following criteria:

- **stakeholder and community feedback/concurrence,**
- **design, construction, operations and maintenance (O&M) cost impacts,**
- **schedule impacts,**
- **passenger experience,**
- **right-of-way (ROW) assessment, and**
- **effects on utilities.**

Section 3 provides information about the Community Working Groups (CWGs) for the BSVII stations, feedback received from CWG members about the CWG structure, and VTA staff's approach with CWGs to address the Board Referral. Moving forward, for all the CWGs, VTA staff will provide hybrid meetings options starting in July 2024, project documents, quarterly verbal report updated to the VTA BOD about CWG meetings, and quarterly CWG meetings with additional CWG meetings as needed. For 28th Street/Little Portugal CWG, VTA staff intends to retain an external facilitator.

Section 4 is specific to 28th Street/Little Portugal BART Station. This section contains a summary of stakeholder and community concerns and engagement activities (past, current, and future). The community is concerned that the above ground station infrastructure facilities would adversely impact the ability to activate ground floor uses, reducing visibility of the plaza as well as its size. In addition, the community had perceived an impact to the future development potential on the site with station infrastructure facilities moving above ground. It details three options for the station infrastructure facilities, the evaluation process for these options, and plan to formulate a staff recommendation. This recommendation will address stakeholder and community concerns and includes a financial strategy, work plan, and timeline for staff recommendations.

The **primary option** for station infrastructure facilities at 28th Street/Little Portugal is for most of the station infrastructure facilities to be shifted below ground on the east side of the tunnel with specific station

infrastructure elements kept above ground in the area north of the station entrance building. If the primary option is found to be not viable and/or costly through the evaluation process, the following alternative options are being considered for the station infrastructure facilities at 28th Street/Little Portugal:

- **Alternative Option A (Below and Above Ground):** Shifts approximately half of the station infrastructure facilities below ground while keeping the remaining station infrastructure facilities above ground in the area north of the station entrance building; and
- **Alternative Option B (Above Ground):** All station infrastructure facilities would remain above ground in the area north of the station entrance building.

Section 5 is specific to Diridon BART Station. This section contains a summary of stakeholder and community concerns and engagement activities (past, current, and future), which include concern that the BART station is located too far from the existing Diridon Caltrain Station and not sufficiently integrated with the future Diridon Intermodal Station as well as concern that locating station infrastructure facilities above ground might adversely impact potential future transit-oriented development and the activation of ground-floor public space, especially along Montgomery and Cahill Streets. It describes four options for the station infrastructure facilities, three options for the Diridon Intermodal Station Direct Connection, process to evaluate the options, and plan to develop a staff recommendation. This recommendation will address stakeholder and community concerns and includes a financial strategy, work plan, and timeline for staff recommendations.

There are three primary options to locate the station infrastructure facilities below ground, which are:

- **Primary Option A:** Station infrastructure facilities would be located below ground, with elements above grade as part of the station entrance building, the potential for an 85-foot TOD, and a standard roof on the station entrance building.
- **Primary Option B:** Station infrastructure facilities would be located below ground, with elements above grade as part of the station entrance building, the potential for an 85-foot TOD, a 25-foot Emergency Vehicle Access (EVA), and a standard roof on the station entrance building.
- **Primary Option C:** Station infrastructure facilities would be located below ground, towards Cahill Street, with elements above grade as part of the station entrance building, the potential for an 85-foot TOD, and standard roof on station entrance building.

If the primary options are found to be not viable and/or costly through the evaluation process, the following alternative option is being considered for the station infrastructure facilities at Diridon:

- **Alternative Option (Above Grade):** All station infrastructure facilities would be located above ground (current design), with future rooftop open space on top of the station entrance building.

The three options for the Diridon Intermodal Station Direct Connection at Diridon are:

- **Option A:** Above ground passenger connection from the Diridon BART station entrance building to the Diridon Intermodal Station.
- **Option B:** Underground passenger connection from the Diridon BART station entrance building to the Diridon Intermodal Station.
- **Option C:** Underground passenger connection from the BART tunnel to the Diridon Intermodal Station.

Section 6 and **Section 7** are specific to Downtown San José BART Station with **Section 6** focusing on design aesthetics and **Section 7** focusing on the south entrance.

- **Section 6** provides a summary of stakeholder and community concerns from past engagement, current and future engagement, design elements for consideration, and the approach and timeline to partner with stakeholders and the community to address the VTA BOD direction in the Board Referral.



- **Section 7** provides a summary of stakeholder and community concerns, details past engagement activities, and the proposed work plan between VTA staff and City of San José (CSJ) to advance decision making for the south entrance.

Section 8 describes VTA staff's plan and next steps to develop RR No. 1. This section outlines the timeframe of milestones and engagement activities with stakeholders and community including VTA BOD, BSVII Oversight Committee, and CSJ.

2 CONTEXT

VTA's BART Silicon Valley Phase II Extension Project (VTA's BSVII) is a six-mile, four station extension of Bay Area Rapid Transit (BART) service from the Berryessa/North San José Station through downtown San José to the City of Santa Clara. This final extension will achieve the vision set by residents of Santa Clara County over 20 years ago and deliver the final missing segment for ringing the Bay with high-frequency, high-capacity rail transit.

2.1 PURPOSE OF THE REFFERAL REPORT

Recent stakeholder and community concerns have been raised regarding the current design of VTA's BSVII facilities, with particular focus on the three stations within the CSJ. Report Revision No. 0 (RR No. 0) identifies VTA's strategy for addressing feedback and concerns related to the design of the 28th Street/Little Portugal BART Station (28th Street/Little Portugal) and Diridon BART Station (Diridon), and the desire for a south entrance at the Downtown San José BART Station (Downtown San José). RR No. 0 details the menu of options being evaluated for each station to address stakeholder and community concerns.

RR No. 0 outlines a work plan, financial strategy, and associated timeline detailing VTA's recommended approach to resolving stakeholder concerns and has been developed in response to the Board Referral issued by the VTA Board of Directors (BOD) on May 2, 2024. The Board Referral can be found in **Appendix A**.

2.2 BACKGROUND

In 2022, VTA facilitated Station Design Refinement meetings with staff from CSJ, BART, and SPUR to address concerns about station designs, primarily related to transit-oriented development (TOD) and passenger experience. Based on feedback from these meetings, VTA made refinements to the Downtown San José and Diridon station designs including:

1. Placing station infrastructure facilities (previously referred to as Back-of-House facilities) at Diridon Station under the station entrance building to create more space for unencumbered TOD south of the station entrance building on the remaining parcel;
2. Increasing the size of the station entrance building shaft at the Downtown San José Station to reduce the number of escalator switchbacks and enhance the passenger experience; and
3. Adding a knock-out panel and mezzanine extension in the tunnel to facilitate a future south entrance at the Downtown San José Station.

A knock-out panel and mezzanine extension at Diridon Station were not included in VTA's refinement effort, as the future entrance location for the Diridon Integrated Station Concept (DISC) project had not been finalized, and the DISC design team had not specified the panel's placement.

As design of the stations advanced from 30 to 60 percent design, the BSVII design team coordinated with BART along with station design advancement identified various station infrastructure facilities that need to be located above ground. In 2023, VTA conducted value engineering for the station designs to improve cost efficiencies, refine preliminary designs from the 2022 Station Design Refinement process, and implement BART's preferred location for station infrastructure facilities, focusing on minimizing operational costs and expanding safety program space. For example, station infrastructure facilities house large electrical and mechanical components weighing several tons each. Above ground facilities facilitate easier access to these large electrical and mechanical equipment components and allow for easier removal at street level.

In November 2023, the station configuration changes that resulted from the value engineering effort were shared with CSJ staff liaisons to the BSVII Project, including staff from the Departments of Transportation and Public Works. This meeting took place in preparation for discussions on station and access facilities with various CSJ departments in December 2023. VTA staff planned to share the station configuration changes with VTA's BSVII Oversight Committee in November 2023, but due to higher priority agenda items, the updates were shared at the February 8, 2024 Oversight Committee meeting. The station configuration changes were also shared with VTA's BSVII Community Working Groups on February 6-8, 2024. However, the station configuration changes raised new concerns for 28th Street/Little Portugal Station stakeholders, including a potential reduction of the station plaza size, limited on-site development opportunities, and challenges in activating ground-floor public spaces due to above ground station infrastructure. VTA staff have been and continue to engage with community members to collaboratively address these concerns.

2.3 BOARD OF DIRECTORS' DIRECTION

The Board Referral dated May 2, 2024, from the VTA BOD, directed VTA staff to address the following five items and can be found in **Appendix A**. The Board Referral directed VTA staff to:

1. Provide a written report within 60 days to the VTA BOD on the public feedback and stakeholder concerns raised since December 2023 related to the 28th Street/Little Portugal, Downtown San José, and Diridon BART Stations for VTA's BSVII. In addition to a comprehensive summary detailing the concerns raised, the written report shall include a menu of options to address stakeholder concerns, and should provide a work plan, financial strategy, and associated timeline detailing staff's recommended approach to resolving the concerns.
2. Continue working with the CSJ and Community Working Groups (CWGs) to incorporate the following station design refinements:
 - o **28th Street/Little Portugal BART Station**
 1. Locate Back-of-House Facilities (BOH) (also referred to as station infrastructure facilities) underground, and under the plaza
 2. If all options to locate the BOH underground have been exhausted, then:
 - a) Provide an in-depth presentation to the Oversight Committee and the 28th Street/Little Portugal CWG on the reasons why; including a comparison with Downtown San José BART Station and its BOH location;
 - b) Minimize the BOH footprint next to the station headhouse (also referred to as station entrance building) and provide an in-depth presentation to the Oversight Committee on what is included in that BOH facility and what factors influenced their placement next to the headhouse versus the North Vent; and
 - c) Design the station and surrounding transit-oriented development with a plaza that maximizes opportunities for community activation, recreation, and access to open space.
 - o **Downtown San José BART Station**
 1. Within the next 3 months (by Month), establish a project plan to realize the Downtown San José south entrance separate from the federal project, from start to finish, that would result in the entrance opening concurrently with the overall BSV Phase II Project Revenue Service start date.
 2. Within the next 6 months (by Month), identify an initial phase of work for the Downtown San José south entrance project that could commence immediately thereafter, and program staff and funding (separate from the federal BSV project) to carry out the south entrance through the environmental clearance phase.

3. Within the next 12 months (by Month), identify a long-term funding strategy for the Downtown San José south entrance.
4. Ensure that both the north and south headhouses strive for the highest level of iconic, context sensitive design, including art and design cues from the surrounding Downtown community.
- o **Diridon BART Station**
 1. Locate BOH underground.
 2. If all options to locate the BOH underground have been exhausted, then:
 - a) Provide an in-depth presentation to the Oversight Committee and the Diridon and Downtown CWG on the reasons why; including a comparison with the Downtown San José BART Station and its BOH location; and
 - b) Line the edges of the station infrastructure facilities with active, ground-floor space such as retail along its sides facing Cahill Street and Montgomery Street; if this cannot be accommodated, provide an alternative plan for how the above-ground BOH facility will be activated at street level and vet this option with City, Oversight Committee, and CWG.
 3. Redesign the tunnel to facilitate a future passenger connection to the Diridon Intermodal Facility; specifically, work with the DISC team to identify a location for a knock-out panel and include the knock-out panel and mezzanine extension in the BSVII project design.
3. Return to the BOD with an exhaustive report of options for a Downtown San José BART Station south entrance.
4. Reinstate the pre-pandemic CWG structure. This should include in-person meetings, dedicated staff/consultant support, and review of appropriate project documents prior to their presentation to the VTA BOD to ensure CWG input can be included in the Board report.
5. Direct administration to provide monthly station design updates to VTA's BSVII Oversight Committee, beginning May 9, 2024.

2.4 REFERRAL RESPONSE PROCESS AND EVALUATION CRITERIA

The referral response process aims to address the requests and items outlined in the Board Referral dated May 2, 2024. The goals of the referral response process are to: 1) address stakeholder concerns regarding the current design of 28th Street/Little Portugal, Diridon and Downtown San José BART stations, as well as the desire for a south entrance at Downtown San José Station, 2) develop and evaluate station options to address stakeholder concerns, and 3) develop a work plan, financial strategy, and associated timeline detailing VTA staff's recommended approach for resolving stakeholder and community concerns.

The referral response process involves extensive technical evaluations and active stakeholder engagement to develop and refine station configuration options. VTA staff will work closely with stakeholders to address their concerns and develop informed station options and recommendations.

The station options will be evaluated using the following criteria:

- **stakeholder and community feedback/concurrence,**
- **design, construction, operations and maintenance (O&M) cost impacts,**
- **schedule impacts,**
- **passenger experience,**
- **right-of-way (ROW) assessment, and**
- **effects on utilities.**

Further details about the station options and their evaluation process can be found in the following sections.

The technical evaluation, combined with stakeholder engagement, will enable VTA to analyze station options and develop staff recommendations to address stakeholder concerns, which are planned to be presented at the September 5, 2024 VTA BOD meeting. VTA staff will also provide monthly station design updates to VTA's BSVII Oversight Committee.

2.4.1 Referral Reports

VTA staff will provide two written reports (Referral Report Revision No. 0 and Referral Report Revision No. 1) to the VTA BOD and BSVII Oversight Committee. These reports will address stakeholder requests and concerns related to the 28th Street/Little Portugal, Downtown San José, and Diridon BART Stations.

Referral Report Revision No. 0 (RR No. 0) represents VTA staff's response plan. RR No. 0 will summarize public feedback and stakeholder concerns raised since December 2023 related to the 28th Street/Little Portugal, Downtown San José, and Diridon BART Stations. It will detail the menu of station options being considered and will outline VTA staff's plan for engaging and evaluating the station options. VTA staff plans to submit RR No. 0 to BOD and the BSVII Oversight Committee by July 2024.

Referral Report Revision No. 1 (RR No. 1) represents VTA staff's evaluation findings and recommendations of options to further explore. RR No. 1 will build upon RR No. 0 by providing additional information for the refined station options, summarizing the findings from the evaluation of station options, and outlining staff recommendations. VTA staff intend to submit RR No. 1 to BOD by September 5, 2024.

3 COMMUNITY WORKING GROUPS

VTA's BSVII CWGs were re-established in 2015 to support the Phase II Santa Clara, 28th Street/Little Portugal, and Downtown-Diridon station areas. CWG members are appointed to represent community organizations. They are tasked with communicating project information to stakeholders in each station area and gathering community feedback to share with the project team.

Prior to the 2020 pandemic, CWGs met in person on a bi-monthly basis or five times a year, with a summer recess. Following the 2020 pandemic, CWG meetings transitioned to a virtual format where meetings occurred quarterly with additional meetings added on an as-needed project milestone basis.

In 2022 and early 2023, a poll of CWG members and agency representatives was conducted to gauge interest in returning to in-person meetings. With 31 responses received, the results were as follows:

- Approximately 65% of responses (20 responses) noted that either in-person or virtual meetings would work;
- Approximately 32% of responses (10 responses) noted they would prefer virtual meetings; and
- Approximately 3% of responses (1 response) expressed the need to return to in-person meetings.

In May 2024, another poll was conducted with each of the three CWGs to gather feedback on the CWG meeting formats. Each CWG received the same four questions in the poll. A summary of the poll by individual CWG is shown in **Table 1. Figure 1 to Figure 3** reveal all three groups' varying commitment to attending in-person CWG meetings.

Table 1: May 2024 Poll Summary by CWG

Poll Question	Downtown-Diridon CWG (15 participants)	28th Street/Little Portugal CWG (12 participants)	Santa Clara CWG (9 participants)
Would you like to resume in-person CWG meetings?	55% said yes.	100% said yes.	17% said yes.
If you would like in-person CWG meetings, how often would you like to attend?	33% preferred an option between in-person and virtual.	Participants were split between the following three formats: option between in-person and virtual, in-person for every meeting, and in-person for every other meeting.	55% preferred an option between in-person and virtual.
This is my preference on the CWG meeting format:	82% like the current meeting format with staff facilitation.	63% would prefer a shift in meeting format towards external facilitation.	100% like the current meeting format with staff facilitation.
Would you be interested in a pre-meeting networking time with CWG members, Project staff, and invited presenters?	55% said yes.	100% said yes.	33% said yes.
Would you like to review Project documents prior to the presentation at the Board of Directors meeting?	100% said yes.	100% said yes.	100% said yes.

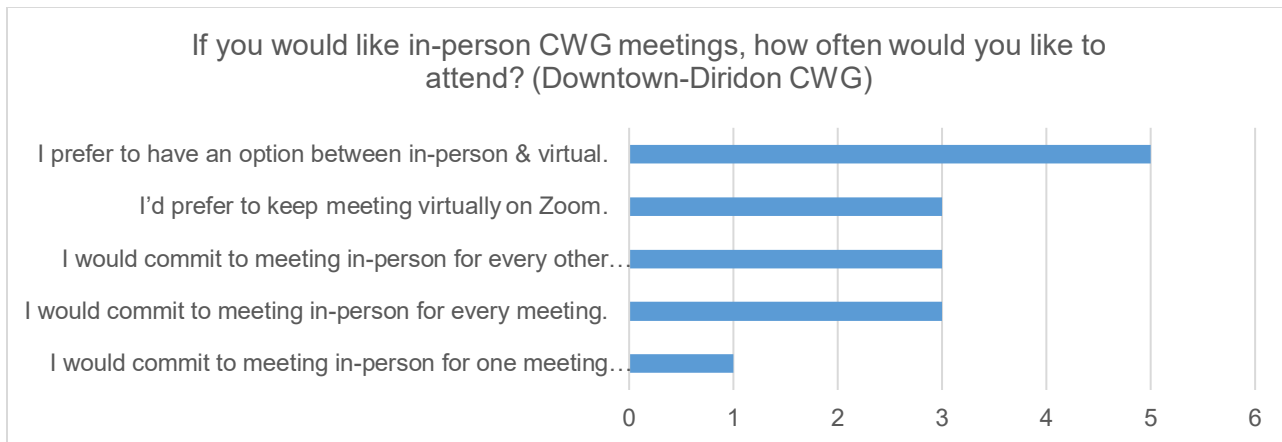


Figure 1: Downtown-Diridon CWG Responses to Frequency of In-Person CWG Meetings (May 2024 Poll)

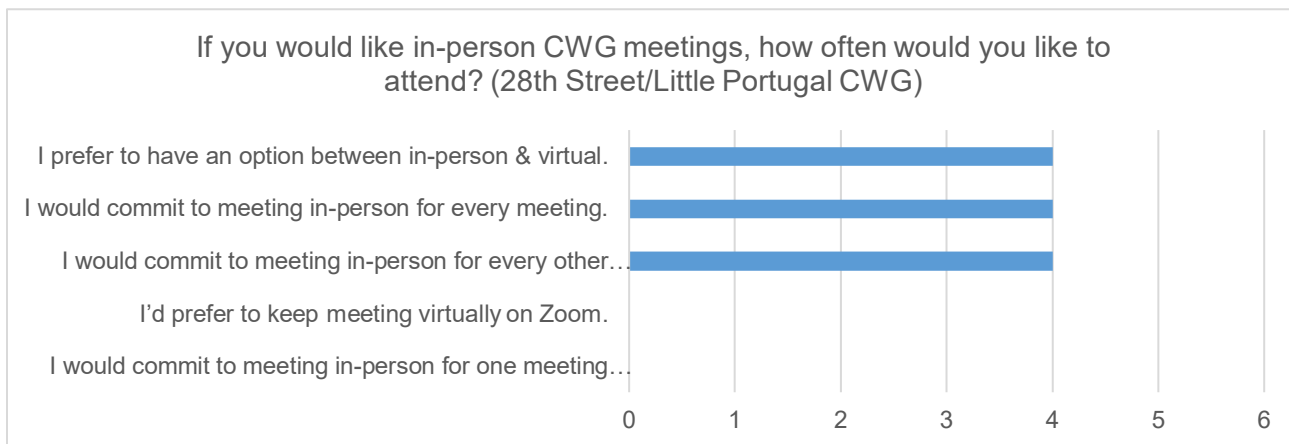


Figure 2: 28th Street/Little Portugal CWG Responses to Frequency of In-Person CWG Meetings (May 2024 Poll)

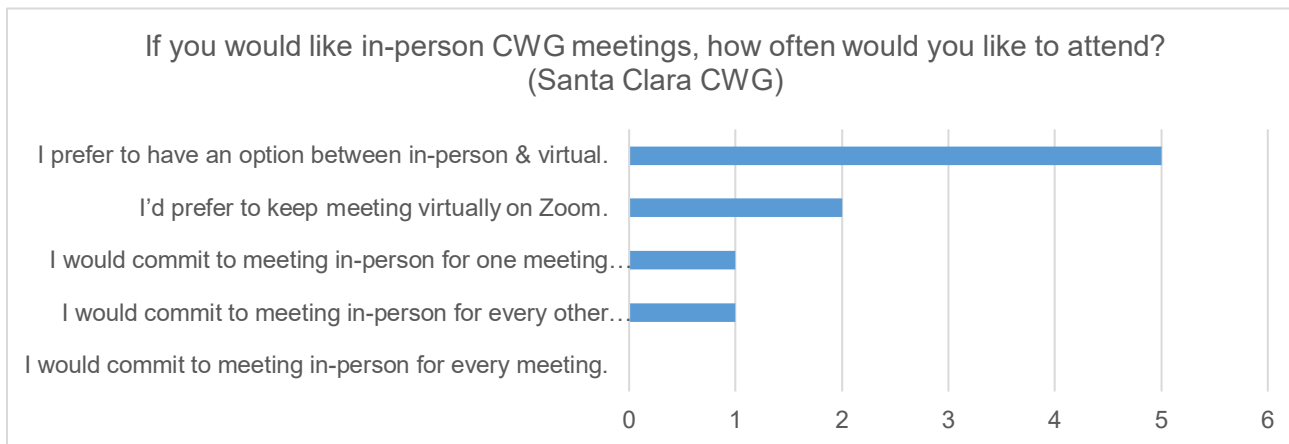


Figure 3: Santa Clara CWG Responses to Frequency of In-Person CWG Meetings (May 2024 Poll)

Next steps for CWG engagement include:

- Offer hybrid meeting options going forward beginning with the July 2024 CWG meetings;
- Hire an external facilitator for 28th Street/Little Portugal CWG meeting;
- Provide CWG members with Project documents related to station design updates, the Construction Transportation Management Plans (CTMPs), Design Review Committee meetings, and other pertinent documents in advance of presenting to the VTA BOD so that the CWG feedback can be shared with the VTA BOD;
- Continue to facilitate quarterly CWG meetings with additional CWG meetings as needed. CWG engagement will occur in July 2024 in addition to the regularly scheduled CWG meetings in September and November;
- Following each quarterly CWG meeting, the BSVII Project team will update the VTA BOD with a verbal report summarizing the updates presented to CWG members.

CWG materials will be shared with the VTA BSVII Oversight Committee via the Board Office prior to all CWG meetings. VTA staff will provide the VTA BOD with updates based on feedback from the CWGs.

4 28TH STREET/LITTLE PORTUGAL BART STATION

The 28th Street/Little Portugal BART Station will be located at the northeast corner of N 28th Street and Five Wounds Lane (see **Figure 4**). The latest station designs include a station entrance building fronted by a station plaza. Additionally, station infrastructure facilities are above ground, located behind the station entrance building. The station is a part of two other concurrent planning efforts lead by VTA and CSJ. The first effort is VTA's Design Development Framework (DDF), which guides a vision for VTA's approximately 13 acres of property around the station (highlighted light blue in **Figure 4**). The second effort is CSJ's Five Wounds Urban Village Plan Update, which is a long-range land use and circulation plan for the communities around the station.



Figure 4: 28th Street/Little Portugal BART Station Map

4.1 SUMMARY OF STAKEHOLDER AND COMMUNITY CONCERNS

The following sections detail the concerns and feedback provided by the community and CSJ for the 28th Street/Little Portugal BART Station. **Table 2** provides a summary of the stakeholder and community concerns raised since February 2024.

Table 2: Stakeholder and Community Concerns

Concerns
Concerns that the above ground station infrastructure facility building will not provide ground-floor activation.
Perceived impact on overall on-site development potential due to above ground station support facility building.
Concern that relocating station infrastructure facilities to above ground has reduced the size of the station plaza.
Need for “eyes on the plaza” through ground floor activation of buildings surrounding the plaza. Also concern that station infrastructure facilities building would be non-activated space, impacting the perception of safety in the plaza.

4.1.1 Community Concerns

CWG members expressed concerns about locating the station infrastructure facilities above ground and the resulting effects of it. Specifically, they were concerned that the above ground building would adversely impact the ability to activate ground floor uses, reducing visibility of the plaza. Members perceived this change as negatively affecting the site’s development potential and hindering efforts to create an envisioned station area with a European-style community plaza. Members were also concerned that the above ground location would reduce the plaza’s size.

4.1.2 City of San José Concerns

CSJ staff also expressed concerns about locating the station infrastructure facilities above ground. Like community members, they were concerned the above ground location would adversely impact potential development and urban design. They noted that if station infrastructure facilities are not activated, it could create an unpleasant user experience when people walk by.

4.2 ENGAGEMENT

4.2.1 Past Engagement (December 2023 - June 2024)

4.2.1.1 Community Engagement

February 7, 2024 CWG Meeting

The community learned about the above ground placement of the station infrastructure building during the February 7, 2024 28th Street/Little Portugal CWG meeting when the BSVII Project team shared updated site plans. During this meeting, CWG members expressed concern about the size of the station infrastructure facilities building and how it could impact development and opportunities on the site. The week following the CWG meeting, Tom Maguire, (Chief Megaprojects Officer), Jessie O’Malley-Solis (Director of Real Estate & Transit Oriented Development/Communities (TOD.C)), and BSVII staff met with Davide Vieira to discuss his concerns and further explain the station configuration change and its relation to the future development on the site.

In late February 2024, after the meeting with VTA, community member Davide Vieira initiated a petition and shared the following concerns in an email to the Friends of the Five Wounds Trail:

- *Back as far as 2006, the community has been planning the urban village on top of this subway station. First came the BART Station Area Community Concept Plan (2010) and then the original and multiple updates of the Five Wounds Urban Village Plan (2013, 2018, 2020, 2022).*

- *VTA has suddenly changed course and removed the machines that run the station entrance building from underground in the footprint of the building. Instead, the machines will exist in a 20,000 sq. ft. above ground building situated next to the station entrance building.*
- *The current VTA station layout deviates drastically from the community's vision for a European-style plaza surrounded by low-rise commercial and services as depicted in the current Five Wounds Urban Village Plan.*

In direct response to the petition, and with a commitment to address community concerns, the Project team conducted two in-person workshops in April 2024 with the following key community leaders: Davide Vieira, Terry Christensen, Chris Esparza, Isamar Gomez and Joan Rivas Cosby. BSVII Project staff, VTA TOD.C staff, the DDF team, CSJ staff, and Mayor Mahan's staff also attended the meetings.

Community Listening Session Meeting

The first meeting occurred on April 8, 2024, and was held at the BSVII Project office. The meeting was facilitated by Winter Consulting and was structured to be a listening session where community leaders could share their perspective on the new station configuration. The meeting agenda included discussion of shared goals for the station area and a path forward. During the discussion, community leaders shared the following key concerns:

- They felt blindsided and disregarded by station configuration changes.
- They want to be included in discussions concerning the station and station area layout/design.
- They want to see ongoing collaboration and engagement, ensuring community input is incorporated into future station planning.
- They understood funding challenges of the project but emphasized the importance of prioritizing community needs.
- They expressed concern about design changes, noting that space for the newly placed station infrastructure facilities in combination with the station entrance building shape changing from circular to square, would impede on the community's vision for a "European Plaza".
- They highlighted the importance of maintaining cultural sensitivity in the area, with a request to ensure designs reflect community priorities and values. The community supported the previous circular station entrance building design because it resembled the Five Wound Historic Portuguese National Parish's two round domes on top of the church.

Community Station Workshop Meeting

A second meeting was held on April 18, 2024, as a follow-up to the April 8, 2024 meeting. The purpose was to collaborate on developing a common vision for the station and address project constraints. The goals of the meeting were to share accurate and relevant information about existing constraints and establish that compromise would be necessary, meaning that the original vision will need to be adapted. It was also essential to ensure that the community was included in planning discussions to determine the necessary adaptations of the original vision. During the meeting:

- The Project team shared the history of project development and engineering requirements that resulted in the station infrastructure facilities being moved to the above ground location.
- The VTA TOD.C team and DDF team also shared their analysis of the effective sizing of plazas and open space. The Project team shared six conceptual layouts of the station infrastructure facilities and station plaza, shown in **Figure 5**.
- Community leaders had an opportunity to review large versions of the site plans and discuss the details of each layout with the Project team, VTA TOD.C staff, the DDF team, and CSJ staff. The key community leaders shared that their preferred concept was Gateway Plaza, followed by Gateway Plaza with Pavilion and Between the Paseos. They preferred to have the plaza located at the corner

of 28th Street and Five Wounds Lane and wanted any planned above ground station infrastructure facilities building to include activation on as many sides as possible.



Figure 5: Station Conceptual Layout Options Shared with Community Leaders

4.2.1.2 City Engagement

Engagement with CSJ in November and December 2023 about the Station Refinements and Value Engineering efforts is described above in **Section 2.2**. Engagement with CSJ and community leaders in April 2024 is described above in **Section 4.2.1.1**.

The BSVII Project team continued to hold 60-minute bi-weekly meetings, and additional meetings as needed, with CSJ's Departments of Transportation, Planning, and Public Works.

On May 8, 2024, VTA's BSVII and BART staff held a tour of the station infrastructure facilities at the Milpitas BART Station with CSJ, BART, Caltrain, and VTA's BSVII Project team. The tour provided a great opportunity for everyone to see firsthand the required station infrastructure facilities, understand the space required for each based on their function, and observe their placement within the station entrance building and additional buildings on the station site. All attendees were able to ask BART staff questions about the different facility needs and how they compared to those at the Phase II stations. The BART and BSV II team noted how the Phase II infrastructure facilities must provide ventilation for the deep tunnel and that as a result, the ventilation facilities at the Phase II underground stations will be larger than those at Milpitas.

During the May 13, 2024 bi-weekly meeting with CSJ following the Milpitas BART Station tour, the BSVII Project team walked through a 3D model of the three San José stations. The team shared outlined where the various facilities observed at the Milpitas BART Station would be placed in each of the Phase II stations. The City asked questions about what equipment could be located underground and what equipment could be relocated to the North Vent Shaft area. The BSVII Project team shared that they would address the City's questions through the referral process (described in **Section 2.4**). Subsequent meetings with CSJ have been focused on this process, as well as the plan and schedule for upcoming months.

4.2.2 Current and Future Engagement (July 2024 - December 2024)

The following sections detail current and future engagement approach to address stakeholder and community concerns described in **Section 4.1. Table 3** provides a summary of the stakeholder and community concerns raised along with the proposed approach for addressing those concerns.

Table 3: Stakeholder and Community Concerns and Proposed Approach for Addressing Concerns

Concerns	Approach for Addressing Concern
Concerns that the above ground station infrastructure facility building will not provide ground-floor activation.	<p>Project is exploring different configurations of the station infrastructure facilities that include undergrounding part or most of the facilities, this to reduce the facilities' overall building footprint above ground.</p> <p>Project is also working with the DDF Team on how to provide space for future development on one or two sides of station infrastructure facilities building if they remain above ground.</p>
Perceived impact on total on-site development potential due to above ground station support facility building.	Project staff partnered and will continue to partner with the DDF Team who has been able to maintain overall development potential on the site.
Concern that movement of station infrastructure facilities to above ground resulted in a reduction in size of the station plaza.	Project has and will continue to share with the community how the plaza size is consistent with both the plaza previously envisioned and other plazas with similar purpose.
Need for "eyes on the plaza" through ground floor activation in the buildings that surround the plaza and concern that station infrastructure facilities building would be non-activated space and impact the perception of safety in the plaza.	Project staff are working with the DDF Team on how to activate the plaza with kiosks, as well as how to provide space for future development on one or two sides of station infrastructure facilities building if they remain above ground.

4.2.2.1 CWG Engagement

CWG members will have an opportunity to provide their input on the station options during the July and August CWG meetings. At the July CWG meeting, CWG members will have an opportunity to review the preliminary station concept options and evaluation findings. CWG members will also be given an opportunity to provide feedback on the station options and evaluation process (e.g., evaluation criteria). VTA staff will take this feedback and further refine the station options and evaluation of options. At the August CWG meeting, CWG members will be able to review refined station concept options and evaluation findings. CWG members will be given an opportunity to provide feedback on the refined station options and evaluation findings. VTA staff will utilize this feedback to further refine the station options, evaluation findings, and develop staff recommendations.

4.2.2.2 Community Engagement

The Project team will host an in-person community meeting in August to allow all community members to provide feedback on the station concept alternatives. The BSVII Project team will advertise the meeting a month in advance and will invite the community with an email invitation from BSVII, with the hope that key community leaders would leverage their networks to get the word out about the event, as well as through postcards distributed to properties within 1 mile of the station. During the meeting, the BSVII Project team will share the concept alternatives, staff recommendations, and request feedback from the group. The goal of the meeting is to bring the community along in the process of selecting the preferred alternative.

4.2.2.3 City Engagement

VTA is repurposing existing program-wide VTA BSVII-CSJ coordination meetings to discuss and evaluate the implications, impacts, and potential for revisions to station infrastructure facility configuration. The goal of these meetings will be to review and come to consensus about design decisions. VTA BSVII and CSJ are anticipated to meet bi-weekly. The prior bi-weekly meeting series included Departments of Transportation, Planning, and Public Works. Moving forward, staff from Transportation and Planning Departments are required as active participants, and staff from the Department of Public Works and the Office of Economic Development are anticipated to join as well. Any additional meetings will be scheduled as needed.

4.3 STATION OPTIONS

The **primary option** for station infrastructure facilities at 28th Street/Little Portugal is for most of the station infrastructure facilities to be shifted below ground on the east side of the tunnel. However, specific station infrastructure elements would still be kept above ground in the area north of the station entrance building. There would be a plaza located at the corner of 28th Street and Five Wounds Lane. This design aims to maximize the use of underground space while maintaining required above ground infrastructure elements. **Figure 6** shows a draft sketch of the primary option.

If the primary option is found to be not viable and/or costly due through the evaluation process, the following alternative options are being considered for the station infrastructure facilities at 28th Street/Little Portugal:

- **Alternative Option A (Below and Above Ground):** Shifts approximately half of the station infrastructure facilities below ground while keeping the remaining station infrastructure facilities above ground in the area north of the station entrance building. This option includes ground floor activation in the area north of the station entrance building. There would be a plaza located at the corner of 28th Street and Five Wounds Lane. This design aims to balance the below ground and above ground infrastructure elements, providing a mix of functionality and ground floor activation. **Figure 7** shows a draft sketch of alternative option A.
- **Alternative Option B (Above Ground):** All station infrastructure facilities would remain above ground in the area north of the station entrance building. This design eliminates the need for underground infrastructure and focuses on utilizing the space above ground. There would be a plaza located at the corner of 28th Street and Five Wounds Lane. This design aims to prioritize ease of access and visibility of the station facilities. **Figure 8** shows a draft sketch of alternative option B.

Each of the station infrastructure facilities options presents different approaches to the layout and placement of those facilities. The station options will undergo an evaluation to identify staff recommendations for the project to advance. The evaluation process is described below in **Section 4.4**.



**Figure 6: 28th Street/Little Portugal BART Station –
Draft Sketch of Station Infrastructure Facilities Primary Option (Below Ground)**



**Figure 7: 28th Street/Little Portugal BART Station –
Draft Sketch of Station Infrastructure Facilities Alternative Option A (Below and Above Ground)**



**Figure 8: 28th Street/Little Portugal BART Station –
Draft Sketch of Station Infrastructure Facilities Alternative Option B (Above Ground)**

4.4 EVALUATION OF STATION OPTIONS

The BSVII Project team is conducting an evaluation of the station infrastructure facilities options for the 28th Street/Little Portugal BART Station, as described in **Section 4.3**. The evaluation process considers the following criteria:

- **stakeholder and community feedback/concurrence,**
- **design, construction, and O&M cost impacts,**
- **schedule impacts,**
- **passenger experience,**
- **ROW assessment, and**
- **effects on utilities.**

The technical evaluation, combined with stakeholder engagement, will enable VTA to analyze the station options and develop staff recommendations to address stakeholder concerns. A summary of key findings from the evaluation process will be included in RR No. 1.

4.4.1 Evaluation of Station Infrastructure Facilities and Plaza Options

4.4.1.1 Stakeholder and Community Feedback/Concurrence

VTA staff will engage with stakeholders and the community to discuss the station options and the findings based on the evaluation criteria for each option. Current and future engagement for the station is described in **Section 4.2.2**. Feedback from the current and future engagement activities will be included in RR No. 1.

4.4.1.2 Cost Impacts

Cost savings related to capital construction as well as O&M were previously estimated and provided to CSJ in 2023. As part of the referral process, cost savings calculations will be further refined. VTA staff will evaluate each station option to understand the potential cost impacts to design, construction, and O&M. Findings from the evaluation will be included in RR No. 1.

4.4.1.3 Schedule Impacts

VTA staff will evaluate each station option to understand the potential schedule impacts. Findings from the evaluation will be included in RR No. 1.

4.4.1.4 Passenger Experience

Each station option will be evaluated to understand the quality of passenger experience including urban design. Findings from the evaluation will be included in RR No. 1.

4.4.1.5 Right-of-Way Assessment

VTA staff will evaluate each station option to understand impacts to right-of-way. Findings from the evaluation will be included in RR No. 1.

4.4.1.6 Effect on Utilities

VTA staff will evaluate each station option to understand the potential effect on existing and/or planned utilities. Findings from the evaluation will be included in RR No. 1.

4.5 STAFF RECOMMENDATIONS

The current and future engagement activities and evaluation process, which are explained in **Section 4.2.2** and **Section 4.4** respectively, will help VTA staff determine the recommended station infrastructure facilities option and approach to resolve stakeholder and community concerns at 28th Street/Little Portugal. VTA staff intends to present their recommended option to the BOD and BSVII Oversight Committee in September 2024.

To develop staff recommendations, VTA staff identified different options to address the design updates identified for 28th Street/Little Portugal in the Board Referral. VTA staff will utilize the current and future engagement activities to address stakeholder and community concerns by presenting the station options, refining the station options, informing the evaluation process, and gathering feedback to develop staff recommendations. VTA staff will also research and partner with stakeholders and project partners to understand the financial and schedule implications associated with the station options and develop a financial strategy and work plan for the recommended station option.

A summary of the financial strategy, work plan, and associated timeline for staff recommendations will be included in RR No. 1.

4.5.1 Financial Strategy

It will be necessary to identify funding sources and responsible parties to implement the recommended station option, as there will be additional costs for redesign, construction, and future O&M. There will be a need to identify upfront funding sources to move forward and implement the recommended station option. VTA will work with CSJ and other partners to identify funding source(s) most appropriate to advance staff recommended options. The financial strategy will be included in RR No. 1.

4.5.2 Work Plan

The station options to address stakeholder and community concerns may have impacts to the BSVII project schedule, including extending the time for redesign and future construction activities. Development of a work plan to implement the recommended station option will be developed to address this issue. VTA will work with CSJ and other partners to develop this plan and advance the recommended station option. The work plan for the staff recommended options will be included in RR No. 1.

4.5.3 Recommended Approach and Timeline

VTA staff developed the following approach and timeline to select the recommended station option:

- **May 2024 onwards**
 - Provide updates on station design to BSVII Oversight Committee.
- **May 2024**
 - Identify available options to address the design updates identified in the May 2024 Board Referral.
- **June to July 2024**
 - Solicit feedback from stakeholders and community members on the station options and evaluation process;
 - Complete initial evaluation of station options; and
 - Refine options based on initial evaluation and stakeholder feedback.
- **July to August 2024**
 - Refine station options based on initial evaluation and stakeholder feedback;
 - Complete evaluation of refined station options;
 - Solicit feedback from stakeholders and community members on the refined station options and evaluation findings to develop staff recommendations;
 - Prepare and facilitate a community meeting in August 2024;
 - Develop staff recommended station options; and
 - Develop financial strategy, work plan, and associated timeline for staff recommendations.
- **September 2024**
 - Present recommendation to BOD.
- **September 2024 to December 2024**
 - Continue to engage with public, community members, and stakeholders to inform station design concept; and
 - Advance design development of staff recommendations to integrate with next design submittals.

The recommended approach and timeline will be updated for staff recommendations in RR No. 1.

5 DIRIDON BART STATION

The Diridon BART Station will be located between Montgomery and Cahill Streets, just south of Santa Clara Street (see **Figure 9**). The station is being designed with a ground level station entrance building and a below ground station concourse and platform. A building to house station infrastructure facilities is also planned immediately south of the station entrance building.

The existing San José Diridon Caltrain Station, to the southeast, is a historic railroad station that currently serves Caltrain, Capitol Corridor, Amtrak, and Altamont Corridor Express (ACE) regional train services. The San José Diridon Caltrain Station also supports VTA's transit center with local and regional buses, as well as a VTA light rail station. VTA, Metropolitan Transportation Commission (MTC), Peninsula Corridor Joint Powers Board (PCJPB), CSJ, and the California High Speed Rail Authority (CHSRA) have partnered to develop a vision for a future Diridon Intermodal Station that will accommodate the station's future capacity needs. The future Diridon Intermodal Station is being advanced separately from VTA's BSVII Project with ongoing coordination and collaboration to ensure alignment for delivering a future cohesive station area. The Diridon Station will be at the center of adjacent development, which will transform land surrounding Diridon BART Station into a dense urban environment with multi-use developments that will activate the area.

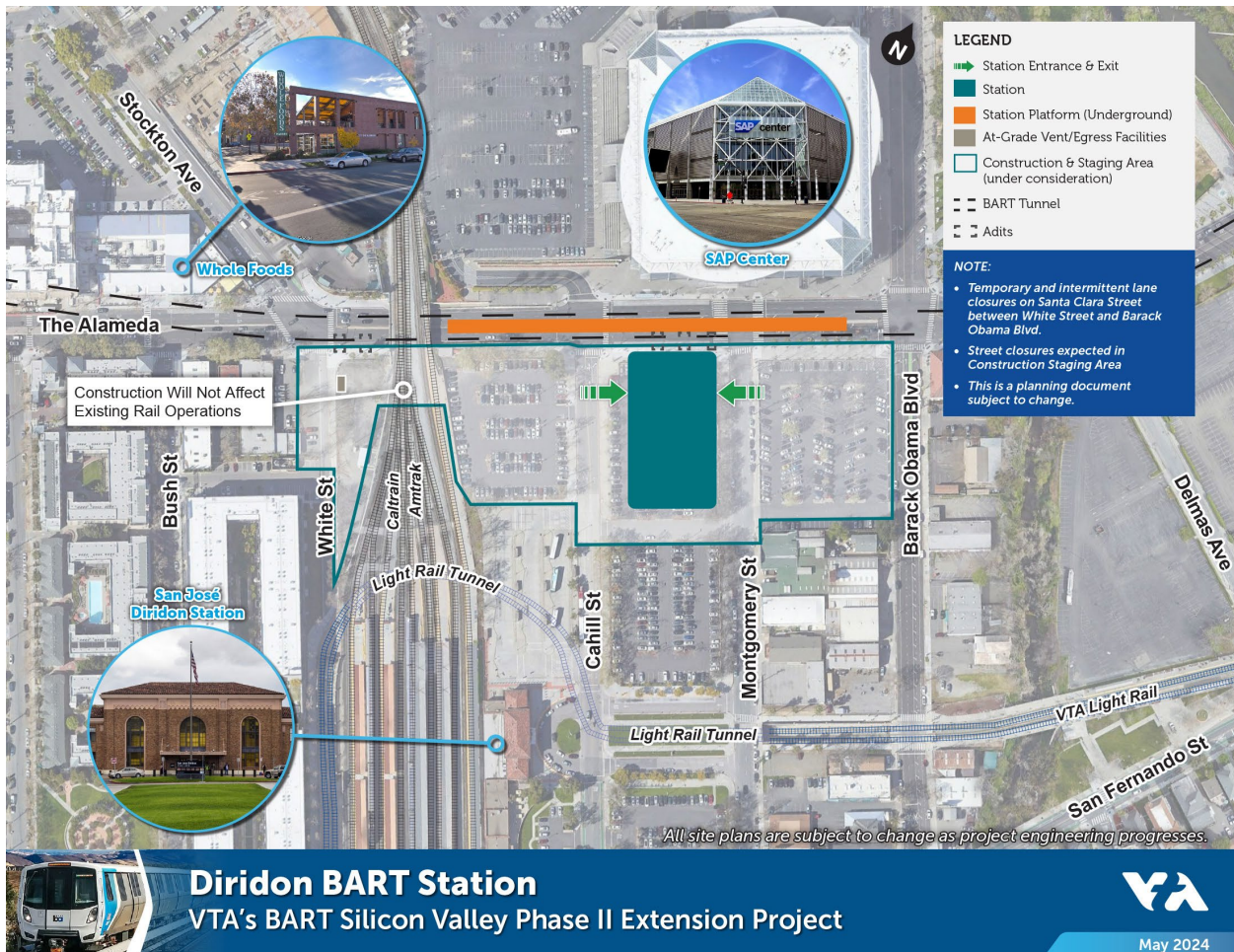


Figure 9: Diridon BART Station Map

5.1 SUMMARY OF STAKEHOLDER AND COMMUNITY CONCERNS

CSJ staff and councilmembers have expressed concerns about the design of the Diridon Stations since 2018. Many of the concerns have been resolved, such as the previous circular roof being updated to a rectangular roof to better accommodate a connection to a future Intermodal Station concourse.

During the environmental clearance for VTA's BSVII Project, a Diridon station – North Option and Diridon station – south option were evaluated. In 2018, CSJ wrote a letter of support to VTA for various options in the environmental document, including support for the Diridon Station – North option, which was approved by the VTA BOD, BART Board, and FTA and is the current location where the station is planned to be located adjacent to Santa Clara Street. Although CSJ supported the north option for the Diridon station, one of the primary concerns is that the BART station is located too far from the existing Diridon Caltrain Station and not sufficiently integrated with the future Diridon Intermodal Station. An underground connection from the future intermodal concourse into the BART tunnel is desired. This connection is assumed to provide for a more seamless transit connection, creating the impression of a unified station, more intuitive wayfinding, and travel time savings.

There was also concern that locating station infrastructure facilities above ground might adversely impact potential future transit-oriented development and the activation of ground-floor public space, especially along Montgomery and Cahill Streets.

Overall, CSJ suggested that careful planning of the station space is necessary due to the potential impact of future adjacent projects, including VTA TOD, Caltrain TOD, Downtown West, and the Diridon Intermodal Station (see **Figure 10**). There is also a need to better understand CSJ's role in programming public space in the station area. Additionally, a 45-foot setback from the back-of-curb of Cahill Street, requested by the DISC Partner Agencies (consisting of VTA, MTC, Caltrain, CSJ and CHSRA) to accommodate a future plaza for the future Diridon Intermodal facility, will limit the availability of ground floor activation and station infrastructure.



Figure 10: Development in Diridon BART Station Area

5.2 ENGAGEMENT

5.2.1 Past Engagement (2017 - February 2024)

5.2.1.1 Direct Tunnel Connection

Since 2017, VTA explored a direct connection between the Diridon BART Station and the future Diridon Intermodal Station through ongoing DISC workshops. In 2019, through the DISC Plan, the Partner Agencies (consisting of VTA, MTC, Caltrain, CSJ and CHSRA) determined that a ground level connection of less than 80 feet across Cahill Street would be an acceptable transfer distance. At the time, VTA recognized the ongoing interest in reviewing alternatives to a ground level connection and has since evaluated the benefits and risks of varied alternatives with the Partner Agencies. This review resulted in a January 2022 memorandum between VTA and the Partner Agencies. The January 2022 memorandum discussed VTA's concerns with an underground connection to the tunnel and station entrance building. In August 2022, VTA presented to the Diridon Technical Directors during the Station Refinement effort (described in **Section 2.2**), including options for a connection for a future second station entrance.

In February 2023, following the Station Refinement effort and better understanding the uncertainties with the Diridon Intermodal Station project and its low level of design, VTA decided not to pursue the Partner Agencies' request for additional infrastructure necessary for a future underground connection. However, VTA

noted that a future connection would not be precluded. This decision was presented to the VTA BOD on February 2, 2023, and discussed with stakeholders in various other forums.

In October 2023, the Diridon Intermodal Station team, now referred to as the Diridon Business Case team, suggested that VTA consider a direct tunnel connection through the center of the Diridon BART Station's future concourse, rather than using the current west ventilation shaft on White Street.

5.2.1.2 Station Infrastructure Facilities

Engagement with CSJ on station infrastructure facilities is primarily described above in **Section 2.2** for the Station Refinements and Value Engineering efforts. In 2020, VTA Real Estate verbally stated that, at most, VTA could provide a 45-foot setback along Cahill Street on VTA property in order to assist with providing for future plaza space associated with the Intermodal Station. To date, VTA has aligned the entry gates with the required 45-foot setback, acknowledging that this restriction limits space for other potential uses and will require CSJ's involvement in programming a new public space.

Through regular coordination meetings, Google expressed the desire to have future ground floor activation along Montgomery Street as this aligns with their plans included in Downtown West to be a primarily pedestrian zone. Through regular coordination meetings, Google expressed the desire to have future ground floor activation along Montgomery Street as this aligns with their plans included in Downtown West to be a primarily pedestrian zone.

The community learned about the above ground placement of the station infrastructure building during the February 14, 2024, Downtown-Diridon CWG meeting when the Project team shared updated site plans. There were no concerns stated by the CWG members.

5.2.2 Current and Future Engagement (March 2024 - December 2024)

Diridon BART Station stakeholder engagement is primarily being implemented through VTA's newly established Diridon Technical Task Force (Diridon Task Force). The Diridon Task Force includes representatives from CSJ, Caltrain, CHSRA, Google, and the Diridon Intermodal Station Business Case Team. The Diridon Task Force is holistically reviewing how adjacent projects in the Diridon area may impact the BART Station footprint based on projects current preliminary designs. Since many Diridon BART facilities systems are interconnected, any design changes to the BART design from nearby projects will require a review to assess potential impacts on BART facilities and capital projects.

Other adjacent projects to the Diridon BART Station include Google's Downtown West (DTW), CSJ's Airport Connector (APC), Caltrain's TOD, California High-speed Rail, and the Diridon Intermodal Station.

5.2.2.1 City Engagement

While CSJ participates in the Diridon Task Force, additional discussions have occurred during the bi-weekly program wide meetings to inform other CSJ staff and gather their perspectives on the reviewed alternatives. In May 2024, CSJ and VTA BSVII staff met four times to discuss the station infrastructure facilities for both 28th Street/Little Portugal and Diridon including a tour on May 8, 2024. The tour provided an opportunity to observe the required station infrastructure facilities firsthand, understand their space requirements based on function, and see how they are positioned. Additional details of the May 2024 meetings with CSJ and VTA staff is described in **Section 4.2.1.2**.

5.2.2.2 Stakeholder Engagement

In addition to the Diridon Task Force, VTA is coordinating with the Diridon Intermodal Station project by presenting the latest Diridon Task Force information at the Technical Directors meetings, which includes technical staff and senior leadership from the Partner Agencies. The Technical Directors meetings, which

were established through the DISC Cooperative Agreement, allow for representatives of the Partner Agencies to receive the latest information and provide direction on the future Diridon Intermodal Station. Topics include funding, procurement, outreach, current project status, upcoming meetings and milestones, and others. The purpose of the Diridon Task Force presentations to the Technical Directors is to ensure that Diridon partner agency staff at various levels receive the information and have an opportunity to provide feedback. To date, three meetings have occurred in May 2024 (May 7, May 14, and May 21). VTA will also continue to participate in the quarterly Diridon Joint Policy Advisory Board (JPAB) meetings to provide updated project information. VTA participated in the May 15, 2024 JPAB meeting to discuss the formation of the Diridon Task Force and will attend the August 2024 meeting.

There is also ongoing engagement with the Downtown/Diridon CWG and the Diridon Design Review Committee (DRC). These meetings have included discussions on aesthetic treatments, architectural finishes, and landscaping for the station facility among other design elements. The Diridon DRC was initially engaged in 2021 with a round of meetings that ended in broad public engagement about the station's look and feel. CWG meetings are planned for July and August 2024, with the next round of Diridon DRC meetings planned to begin in Fall 2024.

5.3 STATION OPTIONS

Station options are being evaluated for both station infrastructure facilities and a direct connection to Diridon Intermodal Station, as these elements are closely interconnected.

There are three primary options to locate the station infrastructure facilities below ground, which are:

- **Primary Option A:** Station infrastructure facilities would be located below ground, with elements above grade as part of the station entrance building, the potential for an 85-foot TOD, and a standard roof on the station entrance building. **Figure 11** shows a draft sketch of primary option A.
- **Primary Option B:** Station infrastructure facilities would be located below ground, with elements above grade as part of the station entrance building, the potential for an 85-foot TOD, a 25-foot Emergency Vehicle Access (EVA), and a standard roof on the station entrance building. **Figure 12** shows a draft sketch of primary option B.
- **Primary Option C:** Station infrastructure facilities would be located below ground, towards Cahill Street, with elements above grade as part of the station entrance building, the potential for an 85-foot TOD, and standard roof on station entrance building. **Figure 13** shows a draft sketch of primary option C.

If the primary options are found to be not viable and/or costly through the evaluation process, the following alternative option is being considered for the station infrastructure facilities at Diridon:

- **Alternative Option (Above Grade):** All station infrastructure facilities would be located above ground (current design), with future rooftop open space on top of the station entrance building. **Figure 14** shows a draft sketch of the alternative option.

Three options are being evaluated for the Diridon Intermodal Station Direct Connection.

- **Option A:** Above ground passenger connection from the Diridon BART station entrance building to the Diridon Intermodal Station. **Figure 15** shows a draft sketch of option A.
- **Option B:** Underground passenger connection from the Diridon BART station entrance building to the Diridon Intermodal Station. **Figure 16** shows a draft sketch of option B.

- **Option C:** Underground passenger connection from the BART tunnel to the Diridon Intermodal Station. **Figure 17** shows a draft sketch of option C.

Each of the options, presented above, offers different approaches to the layout and placement of the station's infrastructure facilities and the direct connection to the Diridon Intermodal Station. The station options will undergo an evaluation to identify staff recommendations for the station infrastructure facilities and direct connection to the Diridon Intermodal Station to advance further. The evaluation process is described in the subsequent section.

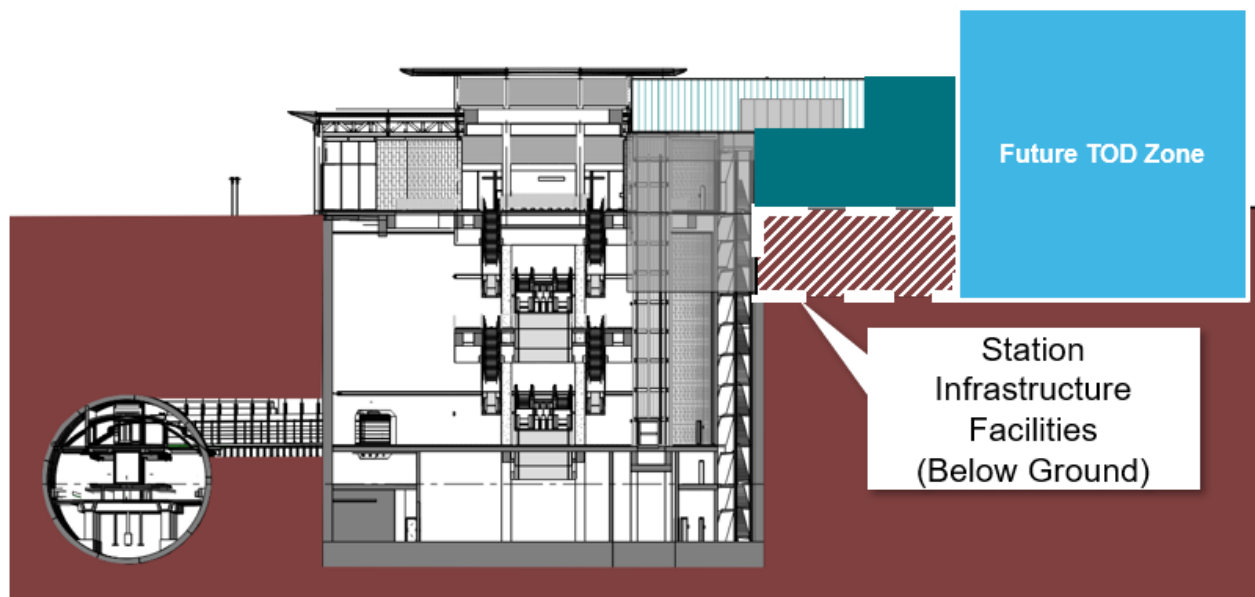


Figure 11: Diridon BART Station – Draft Sketch (Site Plan and Cross Section) of Station Infrastructure Facilities Primary Option A

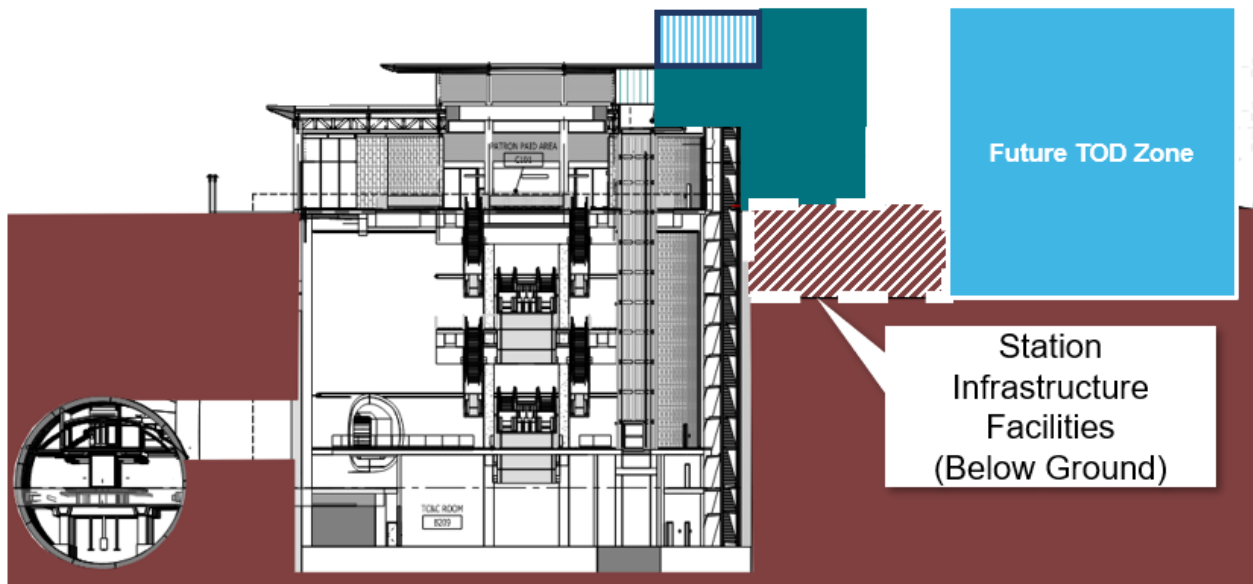


Figure 12: Diridon BART Station – Draft Sketch (Site Plan and Cross Section) of Station Infrastructure Facilities Primary Option B

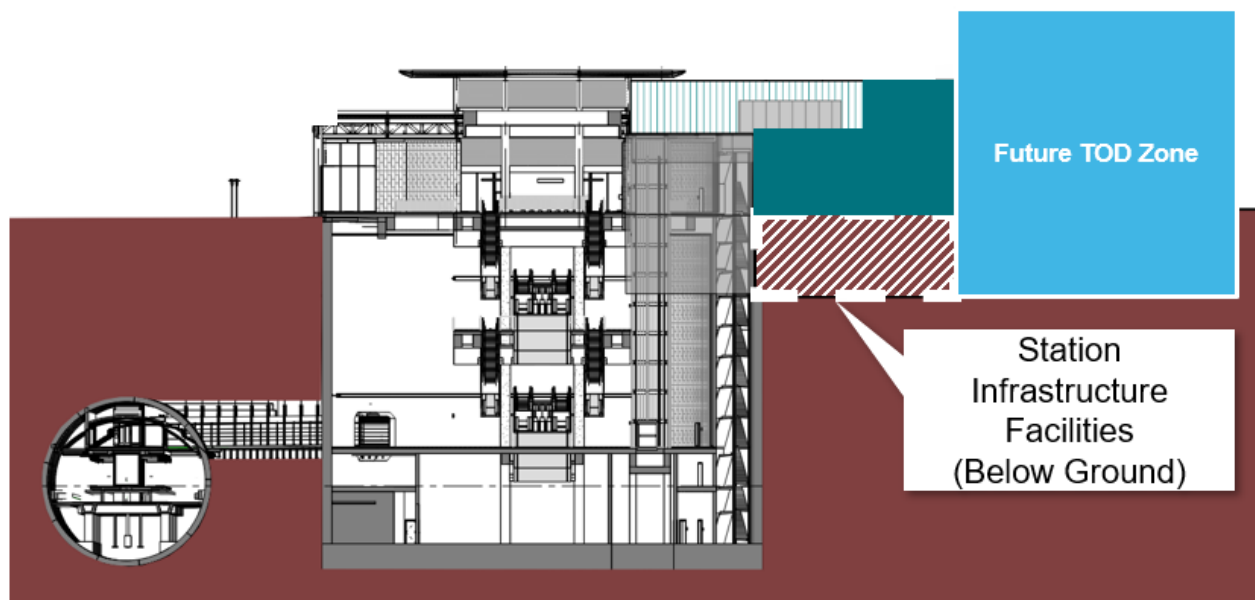


Figure 13: Diridon BART Station – Draft Sketch (Site Plan and Cross Section) of Station Infrastructure Facilities Primary Option C

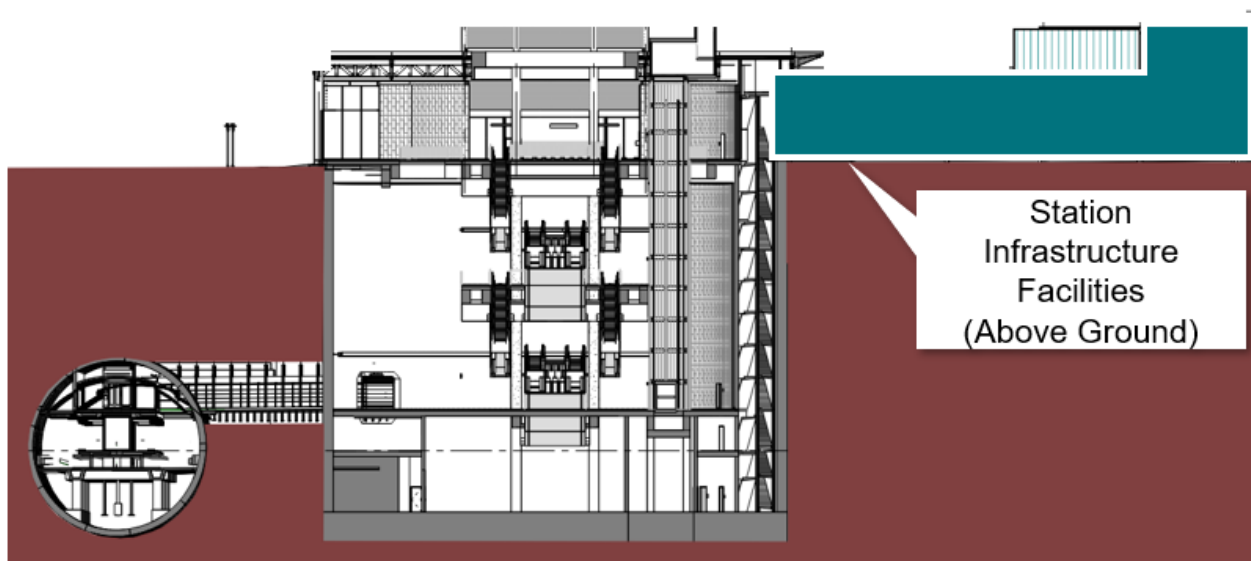


Figure 14: Diridon BART Station – Draft Sketch (Site Plan and Cross Section) of Station Infrastructure Facilities Alternative Option

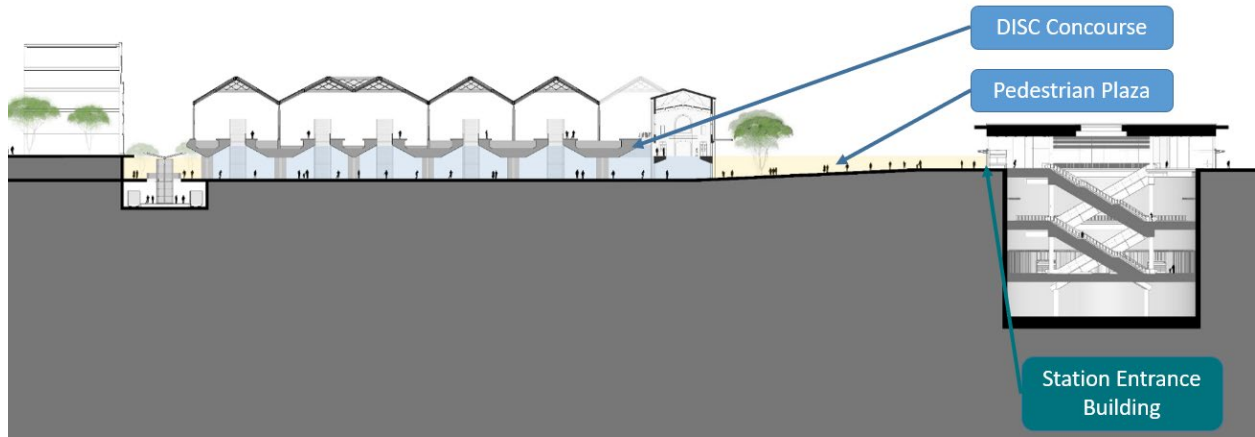


Figure 15: Diridon BART Station – Draft Sketch of Diridon Intermodal Station Direct Connection Option A

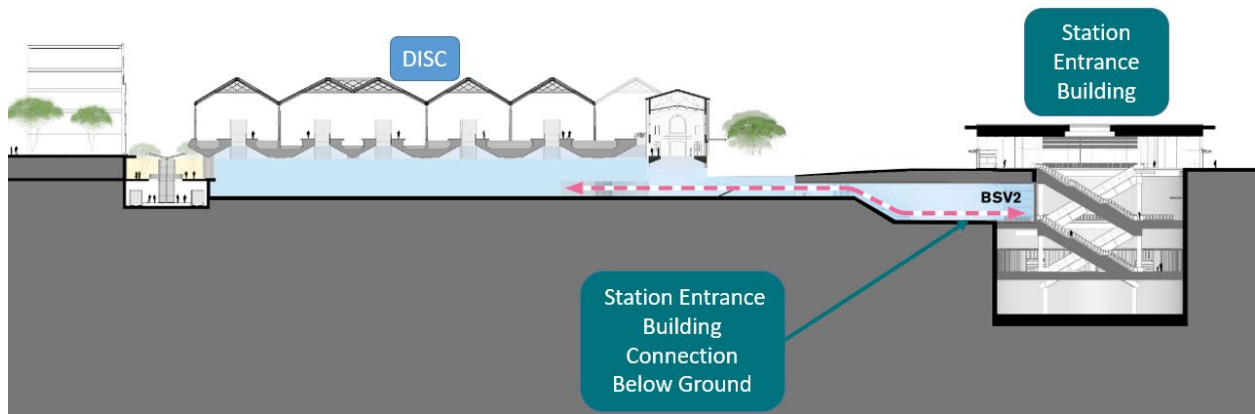


Figure 16: Diridon BART Station – Draft Sketch of Diridon Intermodal Station Direct Connection Option B

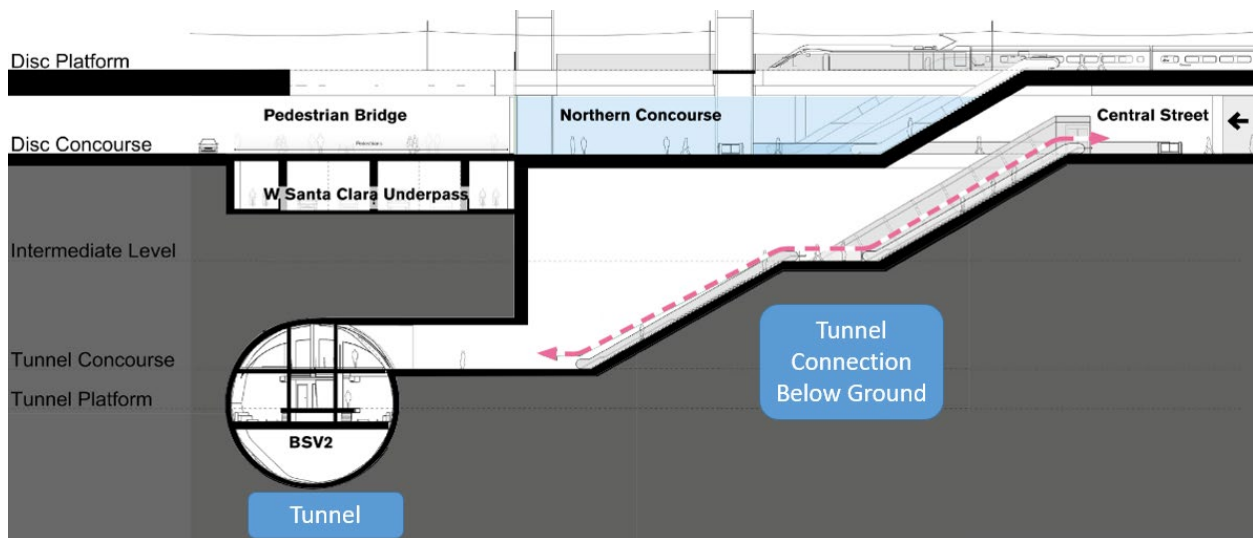


Figure 17: Diridon BART Station – Draft Sketch of Diridon Intermodal Station Direct Connection Option C

5.4 EVALUATION OF STATION OPTIONS

The BSVII Project team is conducting an evaluation of the options for station infrastructure facilities options and Diridon Intermodal Station Direct Connection at the Diridon BART Station, described in **Section 5.3**. The evaluation process considers the following criteria:

- stakeholder and community feedback/concurrence,
- design, construction, operations and maintenance (O&M) cost impacts,
- schedule impacts,
- passenger experience,
- right-of-way (ROW) assessment, and
- effects on utilities.

The goal of this evaluation is to select the option that best meets the project objectives, while providing an optimal experience for transit users and the community. A summary of key findings from the evaluation process will be included in RR No. 1.

5.4.1 Evaluation of Station Infrastructure Facilities Options

5.4.1.1 Stakeholder and Community Feedback/Concurrence

VTA staff will engage with stakeholders and the community to discuss the station options and the findings based on the evaluation criteria for each option. Findings from this engagement will be included in RR No. 1.

5.4.1.2 Cost Impacts

Cost savings related to capital construction as well as O&M were previously estimated and provided to CSJ in 2023. As part of the referral report process, VTA staff will evaluate each station option will evaluate each station option to understand the potential cost impacts to design, construction, and O&M. Findings from the evaluation will be included in RR No. 1.

5.4.1.3 Schedule Impacts

VTA staff will evaluate each station option to understand the potential schedule impacts. Findings from the evaluation will be included in RR No. 1.

5.4.1.4 Passenger Experience

VTA staff will evaluate each station option to understand the quality of passenger experience including urban design, travel time, intuitive path of travel, visual interest and street-level activation. Findings from the evaluation will be included in RR No. 1.

5.4.1.5 Right-of-Way Assessment

VTA staff will evaluate each station option to understand the impacts to right-of-way. Findings from the evaluation will be included in RR No. 1.

5.4.1.6 Effect on Utilities

VTA staff will evaluate each station option to understand the potential effect on existing and/or planned utilities. Findings from the evaluation will be included in RR No. 1.

5.4.2 Evaluation of Diridon Intermodal Station Direct Connection Options

5.4.2.1 Stakeholder and Community Feedback/Concurrence

VTA staff will engage with BART, CSJ, Caltrain, DRC, CHSRA, and the community to discuss the station options and the findings based on the

for each option. Findings from this engagement will be included in RR No. 1.

5.4.2.2 Cost Impacts

VTA staff will evaluate each station option to understand the potential cost impacts to design, construction, and O&M. The team will also evaluate the cost implications of deferred decisions and construction scheduling. Findings from the evaluation will be included in RR No. 1.

5.4.2.1 Schedule Impacts

VTA staff will evaluate each station option to understand the potential schedule impacts. Findings from the evaluation will be included in RR No. 1.

5.4.2.2 Passenger Experience

VTA staff will evaluate each station option to assess its impact on the passenger experience including travel time, urban design, and intuitive wayfinding. Findings from the evaluation will be included in RR No. 1.

5.4.2.3 Right-of-Way Assessment

VTA staff will evaluate each station option to understand impacts to right-of-way. Findings from the evaluation will be included in RR No. 1.

5.4.2.4 Effect on Utilities

VTA staff will evaluate each station option to understand potential impacts on existing and/or planned utilities. Findings from the evaluation will be included in RR No. 1.

5.5 ADJACENT BSVII FACILITY IMPACTS

Several areas within VTA's BSVII Project, including the West Vent Shaft and emergency egress facilities at the Diridon BART station, will be additionally impacted by these station configuration adjustments. Although this area is not specifically included in the workplan outlined in RR No. 0, it is a significant concern for project stakeholders and will be addressed by the ongoing Diridon Technical Task Force. The reconfigured options will ensure that all impacted areas are integrated into a unified recommended solution.

5.6 STAFF RECOMMENDATIONS

The current and future engagement activities and evaluation process detailed in **Section 5.2.2** and **Section 5.4** will assist VTA staff in determining the recommended station infrastructure facilities and direct connection options for the Diridon Intermodal Station. This engagement will also help VTA to effectively formulate an approach to address stakeholder and community concerns. VTA staff intends to present their recommended options and approach to the BOD and BSVII Oversight Committee by September 2024.

A summary of the financial strategy, work plan, and associated timeline for staff recommendations will be included in RR No. 1.

5.6.1 Financial Strategy

It will be necessary to identify funding sources and responsible parties to implement the recommended station options, as there would be additional costs for redesign, construction, and future O&M. The recommended station options, once determined, may be considered a betterment. The Diridon Task Force is evaluating which components must be implemented as part of the BSVII Project and which could be later modified by the Diridon Intermodal Station project. Per the master cooperative agreements between VTA and the Partner Agencies, VTA would negotiate an additional cooperative agreement to formally define the modifications, including definition of a betterment, potential funding sources to cover the additional costs, and the responsible parties for those costs.

Potential funding sources and cost sharing options will be discussed by the Diridon Task Force, in coordination with the Partner Agencies. The financial strategy for the staff recommended approach will be included in RR No. 1.

5.6.2 Work Plan

The Diridon Task Force was established for stakeholders to collaboratively evaluate the design options identified in the Board Referral. The Diridon Task Force has agreed on a process to develop and evaluate station and direct connection options. This process includes:

- Identify and review options with Diridon Task Force. See Table 3 for list of options (completed).
- Develop conceptual plan, section and elevation for each option as basis for evaluation by Diridon Task Force (completed).
- Develop a set of evaluation criteria in collaboration with Diridon Task Force and identify responsibilities to lead each evaluation criterion (completed).
- Distribute concept plans and evaluation matrix for input by Diridon Task Force (completed).
- Compile Diridon Task Force member input and redistribute evaluation matrix for verification by Diridon Task Force (completed).
- Refine concept plans based on collaborative input from Diridon Task Force (in progress).
- Prepare design and construction cost estimates for each concept plan (in progress).

- Consider schedule and phasing for implementation of each concept plan (in progress).
- Update evaluation matrix based on refined design concept plans, cost and schedule.

Once recommended station options are determined, a work plan for the staff recommended approach will be in RR No. 1.

5.6.3 Recommended Approach and Timeline

To develop this recommendation, VTA staff identified different options to address the design updates identified for Diridon station in the Board Referral. VTA staff will utilize current and future engagement activities to address stakeholder and community concerns by refining the station options, informing stakeholders on the evaluation process, and gathering feedback to develop staff recommendations.

VTA staff will also research and partner with stakeholders to understand the financial and schedule implications associated with the station options and develop a financial strategy and work plan for the recommended station option.

The Diridon Task Force was established to engage the agency stakeholders directly in the identification, development, and evaluation of the concepts to address concerns raised in the Board Referral. Through an ongoing series of workshops, the Diridon Task Force collectively evaluated the options, suggested refinements and is considering how to balance the various opportunities and potential constraints.

The following timeline for selecting the recommended station options has been developed:

- **March 2024** – Establish Diridon Task Force to develop and evaluate options.
- **May 2024 onwards** – Provide updates on station design to BSVII Oversight Committee
- **June 2024** – Refine options base on initial Diridon Task Force evaluations and prepare cost estimate and schedule for each option.
- **July 2024** – Develop recommendation for which options to advance for further study and engagement with Stakeholders including DISC Technical Directors, Diridon JPAB, and CWG.
- **August 2024** – Based on stakeholder input, develop RR No. 1 and provide update on tunnel connection at Diridon JPAB August 15, 2024 meeting.
- **September 2024** – Present recommendation to VTA BOD.
- **September to December 2024** – Advance design development of selected option(s) to integrate with next design submittals. Continue engagement with public, community members, and stakeholders on selected station option to inform station design concept.

The recommended approach and timeline will be updated for staff recommendations in RR No. 1.

6 DOWNTOWN SAN JOSÉ BART STATION – AESTHETICS AND HISTORIC CONTEXT

Downtown San José BART Station will be located on Santa Clara Street between Market and First Streets (see **Figure 18**). Ticketing and fare gates will be at street-level while the station's concourse and platforms will be underground. The station will be an urban station, focused on providing access to sustainable and non-motorized modes of transportation. Many of the future riders are anticipated to walk or transfer from VTA light rail and bus service operating on Santa Clara, First, and Second Streets. Various types of bicycle parking are also planned near the station entrances. The station area is designed to integrate with planned on-site development, fostering Downtown's transition into a transit-oriented community. An emergency egress and ventilation facility will be located at the northwest corner of Santa Clara Street and Third Street.

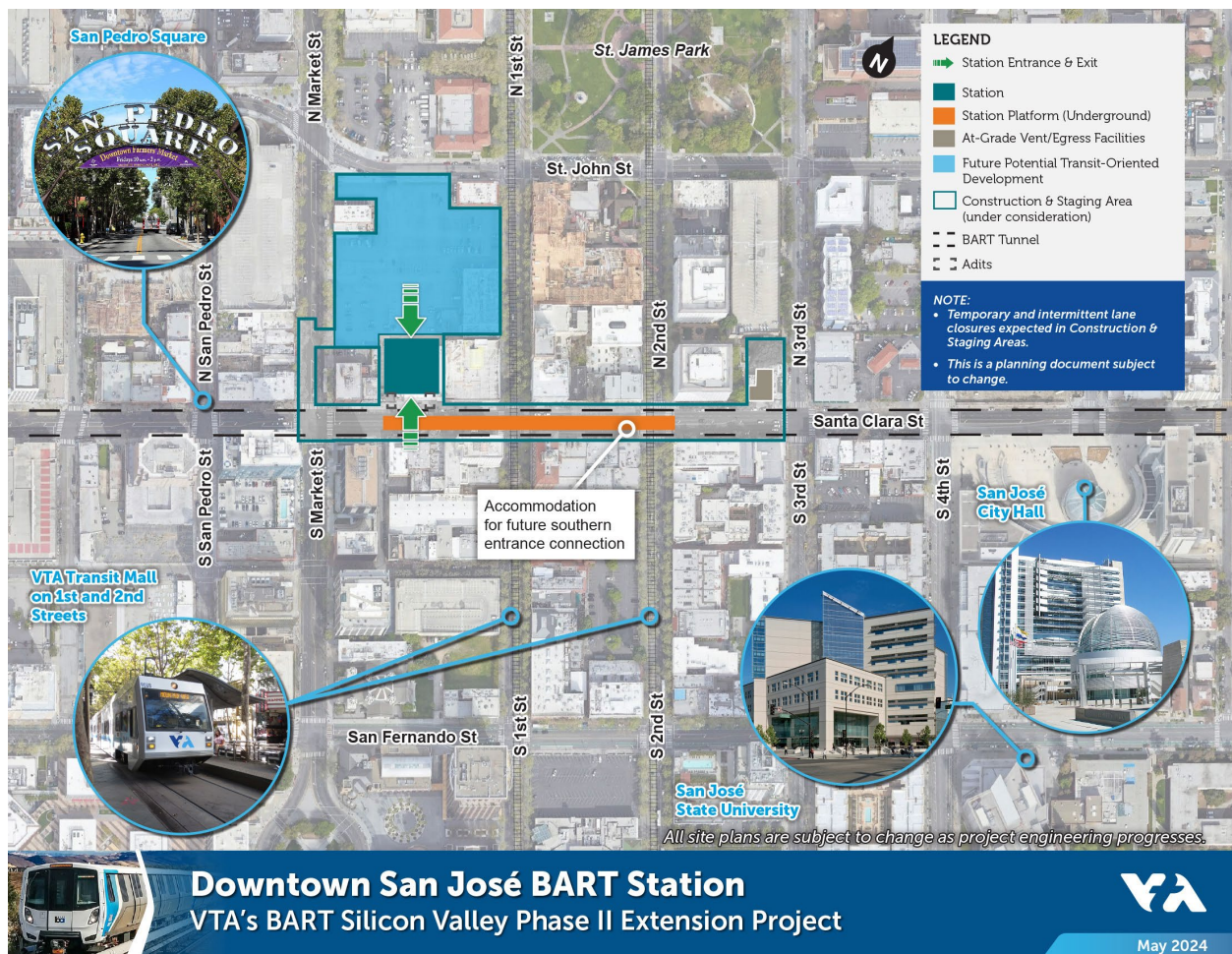


Figure 18: Downtown San José BART Station Map

6.1 SUMMARY OF STAKEHOLDER AND COMMUNITY CONCERNS

In 2021, CSJ expressed concerns about the low-rise design of the station entrance building. The station was originally designed to accommodate up to nine stories of TOD above the station. At the time, it was noted that the seismic requirements for BART facilities were likely to significantly increase the cost of development, this

compared to a TOD being planned adjacent to the station building. In 2022, BSVII worked closely with CSJ and stakeholders to identify opportunities for improved passenger circulation within the station.

During the 2021 Downtown San José Design Review Committee (DRC) meetings, community members expressed a desire for the station to have a 'grand central' feel with a significant civic presence on Santa Clara Street. The Downtown San José DRC was comprised of business, residential, institutional, and City stakeholders. Community stakeholders wanted the station to be a distinctive feature, contrasting with the more uniform environment of the city's downtown area.

In 2022, CSJ leaders, staff, and elected officials raised concerns about the design of the station's vertical circulation, as it would require several switchbacks to access the station concourse(s).

In 2022, BSVII engaged in a station refinement effort with CSJ and SPUR. The effort resulted in changes to the station design, which supported a much simpler vertical circulation, requiring only one switchback. However, the revised design required a larger station shaft to support the longer escalator runs, resulting in a station structure that could no longer accommodate TOD above the station. VTA assured CSJ that the planned TOD could be accommodated elsewhere on the VTA Block, for which VTA has conducted a Design Development Framework (DDF) effort to explore. Following these design changes, CSJ expressed a desire for publicly accessible space above the station building. Notably, the Downtown DDF includes a central plaza with a rooftop garden. Furthermore, there are several other public spaces including Saint James Park (one block north of the station) and Cesar Chavez Plaza (about two blocks south of the station).

In 2023, BSVII refined the station design based on feedback received during the first round of DRC meetings in 2021. The refined design allowed for a taller building, which was more closely aligned with the historic podium level style of downtown San José. To create the desired 'grand central' feel of the station, an arched roof was also added. The updated station design retained the marquee element that provides a strong presence on Santa Clara Street.

6.2 ENGAGEMENT

Since receiving the Board Referral, the BSVII Project team engaged both CSJ and the station's CWG to receive feedback on the station design and aesthetics. At the May 14, 2024 CWG meeting, members expressed enthusiasm towards and support for the latest design, specifically noting an appreciation for the station's open air design and the new, higher arched roof. The BSVII Project team invited the station architect to participate at the May 21, 2024 biweekly BSVII-CSJ coordination meeting to discuss the station design and provide an overview of past DRC efforts. The station architect also provided an overview of public feedback, as well as feedback received at the latest May 14, 2024 CWG. The station design was also included as an agenda topic at the June 10th BSVII-CSJ meeting, which discussed VTA's approach for addressing feedback received to date. Lastly, BSVII held a meeting on June 13, 2024 with key CSJ staff to get their direct feedback and input on the station design ahead of the first DRC meeting for the station, held on June 18, 2024. Over the course of the summer, BSVII staff will continue to engage the community and city staff through regular City meetings, DRC meetings, CWG meetings, as well as in-person and virtual engagement opportunities.

The feedback received from the engagement activities between May 2024 and August 2024 will be included in RR No. 1.

6.3 DESIGN ELEMENTS FOR CONSIDERATION

VTA will engage with the community and stakeholders to discuss and gather feedback on station design elements, including aesthetic treatments, material finishes, landscaping, lighting, and art location.

In 2024, VTA staff will host a series of DRC meetings for the Downtown San José BART station, starting on June 18, to gather input from seven DRC members, representing the community, San José State University, and CSJ.

The June 18 Downtown San José DRC meeting recapped the feedback from the 2021 DRC series, reviewed station configuration updates, discussed station visual elements for DRC feedback, and gathered feedback from DRC members to guide the station design and the DRC process moving forward. At this meeting, DRC members engaged in a facilitated discussion and responded to polling questions to share their thoughts on the current station design, suggest specific art and design elements, and identify aspects for further evaluation in future DRC meetings.

The station design options presented to stakeholders and the community, along with feedback, will be included in RR No. 1.

6.4 RECOMMENDED APPROACH AND TIMELINE

VTA staff will collaborate with stakeholders and the community to develop a station entrance building in Downtown San José, following the VTA BOD's directive from May 2, 2024. The aim is to achieve a highly iconic design that is sensitive to the context and incorporates art and design elements reflective of the Downtown community. VTA staff will engage and solicit feedback from CSJ staff, Downtown San José DRC, Downtown-Diridon CWG, and the community to refine the station design concept.

VTA staff developed the proposed approach and timeline to address the BOD direction:

- **May 2024 onwards** – Provide updates on station design to BSVII Oversight Committee
- **May 2024 to November 2024** – Utilize program-wide meetings with CSJ staff and Downtown-Diridon CWGS to discuss station design updates, solicit feedback on station design elements, and refine station concepts based on feedback.
- **June 2024 to September 2024** – Prepare and facilitate 2024 DRC Round (four meetings) to review and gather feedback on the station design element options to refine the station design concept.
- **September 2024** – Report out to VTA BOD and BSVII Oversight Committee on past engagement activities, feedback received, and approach moving forward to refine station design.

The recommended approach and timeline will be updated in RR No. 1.

7 DOWNTOWN SAN JOSÉ BART STATION – SOUTH ENTRANCE

VTA and CSJ have worked closely to discuss the feasibility of a south entrance to the Downtown San José BART Station through multiple coordination efforts. In 2022, the tunnel's design and configuration was changed from a stacked platform configuration to a center-platform configuration. This change allowed for direct access to the station concourse and platform from the south of the tunnel, which was previously considered infeasible. Due to the lack of environmental clearance for a south entrance and to avoid delaying the advancement of VTA's BSVII Project, VTA refined the tunnel design to enable a future connection, as shown on the site plan (see **Figure 19**). The site plan also shows locations initially identified by VTA and CSJ for evaluation. To proceed with this process, design concepts for these locations must address previously documented issues and avoid adverse impacts on historic and cultural resources.

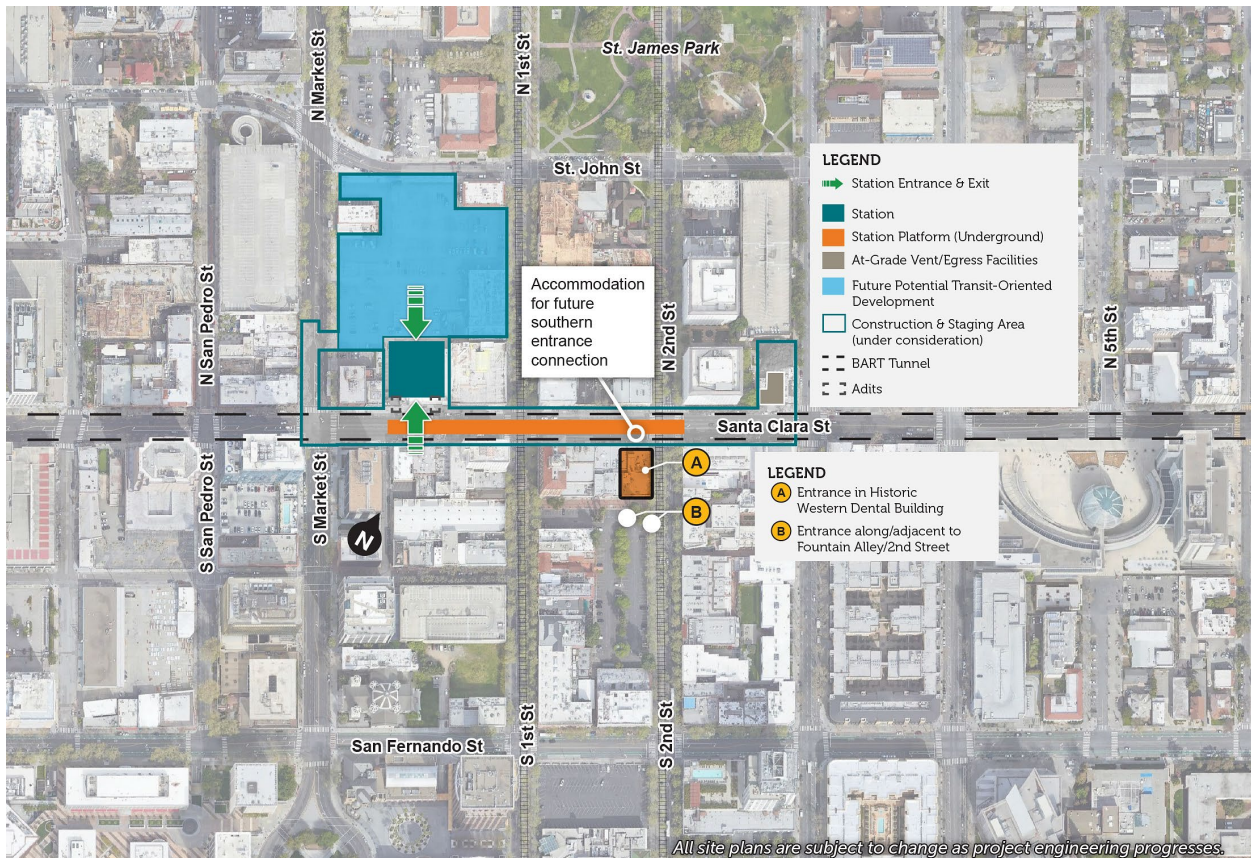


Figure 19: Downtown San José BART Station Site Plan with Future South Entrance Connection

7.1 SUMMARY OF STAKEHOLDER CONCERNS

Various concerns were expressed by stakeholder regarding a south entrance for the station, including an overall desire for access to the station from the south side of Santa Clara Street. Based on a Fehr & Peers analysis of VTA's travel demand modeling, they estimated approximately 60% of trips accessing BART services via light rail would originate south of Santa Clara Street. Furthermore, evaluation of future residential development patterns has shown that most of the growth would occur north of Santa Clara Street.

During the environmental clearance process, San José State University advocated for the “Downtown East” option, which was not selected and expressed dissatisfaction that the second station entrance was not located on Fourth Street. However, environmental clearance documents included a commitment to implementing wayfinding and streetscaping along Santa Clara Street, extending to Fourth Street to serve SJSU students using BART. Conversely, CSJ and most other stakeholders supported the “Downtown West” option which was adopted by the VTA BOD, BART Board and FTA.

CSJ raised safety concerns with designs requiring pedestrians to cross Santa Clara Street, to reach the Downtown San José BART station. To address this, BSVII evaluated multiple safety solutions including pedestrian scramble phases at key intersections. However, VTA bus operations were concerned about the impact pedestrian scrambles would have on their on-time performance for buses operating on Santa Clara Street. VTA estimates that by 2040, up to 26 buses per hour, in each direction, will run along Santa Clara Street.

To facilitate decision making for the development of a Downtown San José south entrance, BSVII and CSJ plan to create a strategy for engaging with FTA and to establish a Downtown San José South Entrance Task Force to support the selection of a viable south entrance option. VTA will also support CSJ in activities related to grant funding application, identifying non-BSVII funding options, and deciding on a preferred south entrance option.

7.2 PAST ENGAGEMENT (2019 - 2022)

In 2019 and 2020, BSVII conducted workshops with CSJ to explore options for a second entrance location south of Santa Clara Street. At the time, the most feasible alternative was a south entrance connected to the secondary entrance on the north side of Santa Clara Street, via a pedestrian undercrossing. This configuration was necessary due to the stacked platform design, which only allowed platform access from the north side of the tunnel. However, workshops concluded there was no viable solution for a south entrance, and the undercrossing/overcrossing options discussed did not meet the cost-benefit analysis criteria.

In 2022, after Kiewit Shea Traylor’s (KST’s) innovation phase, which resulted in the current center platform configuration, BSV re-engaged with CSJ and SPUR to further discuss and evaluate south entrance options. At the time, CSJ’s preference for the south entrance was to utilize the historic Western Dental Building located on the southwest corner of E. Santa Clara Street and S. Second Street. Other options included: 1) A sidewalk entrance along Santa Clara Street, which would significantly reduce sidewalk width and negatively impact pedestrian access; 2) A sidewalk entrance along Second Street, which would cause significant construction impacts to the LRT corridor and station; and 3) An entrance along Fountain Alley, which would require a pedestrian tunnel under Second Street, which would also impact the light rail. This entrance would also require close coordination with the entitled West Bank Development. After further evaluation, using the Western Dental Building was deemed infeasible due to its adverse effects on this culturally significant structure, as identified through the Section 106 process. Additionally, FTA would not permit the use of 4(f) property because feasible and prudent alternatives for avoiding impacts are available. The VTA Environmental Manager confirmed after consultation with FTA that the Western Dental Building option could not proceed with or without federal funding, due to the federal nexus created by connecting the second entrance to an already FTA-funded project. Despite the VTA Environmental Manager’s confirmation from FTA, CSJ has continued to study the Western Dental Building option.

7.3 CSJ AND VTA WORK PLAN

To advance decision making needed for the development of the Downtown San José south entrance, a preliminary timeline and workplan have been developed:

- **June - July 2024** – VTA staff will develop an approach for engaging with FTA. Additionally, BSVII and CSJ will develop an approach for site selection, environmental clearance, funding, design, and construction for the south entrance. A BSVII-CSJ South Entrance Task Force will also be established, which will include decision-makers from the City of San José, BSVII, key stakeholders including SJSU and SJDA, and an external facilitator. Task Force responsibilities may include establishing each party's roles and responsibilities, reviewing and confirming workplans, identifying and evaluating feasible south entrance options, and developing a selection process for the preferred option.
- **August 2024** –The Downtown San José BART Station South Entrance Task Force will evaluate feasibility criteria and identify options for concept development, identify the preferred environmental clearance process, identify potential project delivery methods, and explore non-BSVII funding options for the south entrance.
- **September - October 2024** – VTA will finish concept designs for one or more south station entrance options.
- **October - November 2024** – The Task Force will select a preferred option and develop a funding strategy for environmental clearance of this option. The VTA BOD and CSJ will also work to reach consensus on the preferred option.
- **December 2024** – Following receipt of the FFGA for BSVII, VTA will initiate the CEQA/NEPA environmental clearance process, as well as any third-party agreements needed for advancement of the preferred option.

8 REFERRAL REPORT PROCESS WORK PLAN

RR No. 0 will be submitted to the VTA BOD within 60 days of the initial issuing of the Board Referral. The evaluation of station options will continue to be refined based on further technical analysis, including cost and schedule impacts, passenger experience, ROW assessment, potential environmental impacts, utility effects, and stakeholder feedback. The refined findings from the station options evaluation process will be included in RR No. 1, which is planned for submission to the VTA BOD in September 2024.

Stakeholder engagement with community members, CSJ, BART, CWGs, DRCs, DISC Technical Directors, and other stakeholders will continue for each station. The pre-pandemic CWG structure will be reinstated to include in-person meetings, staff and consultant support, and CWG review of project documents prior to submission of RR No. 1.

The following schedule provides planned key milestones and engagement activities to develop RR No. 1.

- **June to July 2024**
 - Station options development, refinement, and initial evaluation based on evaluation process and feedback from stakeholders and community.
 - June 2024 Engagement Activities
 - Monthly station design update with BSVII Oversight Committee (June 13).
 - Meetings with BART to discuss station design options and evaluation process.
 - Continue program-wide meetings and coordination with CSJ staff.
 - Continue Diridon Task Force meetings and coordination.
 - Meeting with Downtown San José Design Review Committee (June 18).
 - July 2024 Engagement Activities
 - Monthly station design update including RR No. 1 to BSVII Oversight Committee (July 11).
 - Presentation at CWGs to gather feedback.
 - Continue program-wide meetings and coordination with CSJ staff.
 - Briefings with VTA Directors and CSJ Leadership.
- **July to September 2024**
 - Develop staff recommendation (recommended station options, financial strategy, work plan, and recommended approach and timeline) based on refined station options, completed evaluation findings, and feedback from stakeholders and community.
 - August 2024 Engagement Activities
 - Monthly station design update to BSVII Oversight Committee (August 8).
 - Presentation at CWGs to gather feedback.
 - Continue program-wide meetings and coordination with CSJ staff.
 - 28th Street/Little Portugal Community Meeting.
 - Briefings with VTA Directors and CSJ Leadership.
 - September 2024 Engagement Activities
 - Station design update and RR. No. 1 to VTA BOD (September 5).
 - Monthly station design update and RR No. 1 to BSVII Oversight Committee (September 12).



APPENDIX A: VTA BOARD REFERRAL (ISSUED MAY 2, 2024)



APPENDIX A: VTA BOARD REFERRAL (ISSUED MAY 2, 2024)

TO: Santa Clara Valley Transportation Authority
Board of Directors

FROM: Chair Chavez
Director Mahan
Director Davis

DATE: May 2, 2024

SUBJECT: Items of Referral to Staff – Updates to Station Design for VTA’s BART Silicon Valley Phase II Extension Project

REFERRAL:

1. Direct administration to provide a written report within 60 days to the VTA Board of Directors on the public feedback and stakeholder concerns raised since December 2023 related to the 28th Street Little Portugal, Downtown San José, and Diridon Stations for VTA’s BART Silicon Valley Phase II Extension Project. In addition to a comprehensive summary detailing the concerns raised, the written report shall include a menu of options to address stakeholder concerns, and a work plan, financial strategy, and associated timeline detailing staff’s recommended approach to resolving the concerns.
2. Direct administration to perform the design updates as described below.
3. Direct administration to return to the Board with an exhaustive report of options for a downtown San José south entrance.
4. Direct administration to reinstate the pre-pandemic Community Working Group (CWG) structure. This should include in-person meetings, dedicated staff/consultant support, and review of appropriate project documents prior to their presentation to the VTA Board of Directors to ensure CWG input can be added to the Board report.
5. Direct administration to provide monthly station design updates to VTA’s BART Silicon Valley Phase II Oversight Committee, beginning May 9, 2024.

DISCUSSION:

We thank staff for their work thus far in advancing designs for the BART to Silicon Valley Phase II Project (BSVII). We acknowledge the team's dedication towards the delivery of this once-in-a-generation transit extension, which includes three new stations through the heart of San José (28th/Little Portugal, Downtown, and Diridon). These stations will be located in areas of local, regional and statewide significance. Therefore, considering the significance of the station locations, along with the billions in public investments, it is imperative that we design stations that are timeless, vibrant, intuitive, and beneficial to the respective community.

In 2022, VTA facilitated Station Design Refinement meetings with staff from the City of San José, BART, and SPUR, in order to address concerns raised by Board Members, City Councilmembers, City staff, and community members about the station designs, mainly around transit-oriented development (TOD) and passenger experience. As a result of the Station Design Refinement effort, VTA revised the project design, including 1) placing Back-of-House facilities at Diridon Station under the headhouse in order to free up the rest of the parcel for transit-oriented development, 2) enlarging the headhouse shaft at Downtown San José (DTSJ) Station in order to reduce the number of escalator switchbacks and improve the passenger experience, and 3) adding a knock-out panel and mezzanine extension in the tunnel to facilitate a future south entrance at the DTSJ Station. At Diridon, a knock-out panel and mezzanine extension were not included because the location of a future entrance as part of the Diridon Integrated Station project had not been determined yet, but VTA provided that a connection could still feasibly be made at the west end of the platform by a separate future project.

Later in 2023, VTA conducted value engineering on the design to realize cost efficiencies which reversed some of the outcomes of the 2022 Station Design Refinement effort, introducing new concerns at the 28th St/ Little Portugal Station. In recent months, VTA staff have been working with the community to address these concerns, convening discussions to find a collaborative solution for the community and project.

The Board and VTA recognize that the cities, communities, and stakeholders in which the Project stations will be located have a vested interest in the ultimate station design and planned development of the station area. As such, this referral directs VTA staff to continue working with the City of San José and Community Working Groups to incorporate the following updates into the station designs:

28th Street/Little Portugal BART Station

- 1) Locate Back-of-House Facilities (BOH) underground under the plaza
- 2) If all options in attempt to locate the BOH underground have been exhausted, then:
 - a) Provide an in-depth presentation to the Oversight Committee and 28th/LP Community Working Group on the reasons why; including a comparison with Downtown San José Station and its BOH location;
 - b) Minimize the BOH footprint next to the station headhouse and provide an in-depth presentation to the Oversight Committee on what is included in that BOH facility and what factors influence their placement next to the headhouse versus at the North Vent; and

- c) Design the Station and surrounding transit-oriented development with a plaza that maximizes opportunities for community activation, recreation, and access to open space

Downtown San José BART Station

- 1) Within the next 3 months, establish a project plan to realize the Downtown San José south entrance outside of the federal project, from start to finish, that would result in the entrance opening concurrently with the overall BSV Phase II project Revenue Start Date.
- 2) Within the next 6 months, identify an initial phase of work for the DTSJ south entrance project that could commence immediately thereafter, and program staff and funding (separate from the federal BSV project) to carry out the south entrance through the environmental phase.
- 3) Within the next 12 months, identify a long-term funding strategy for the DTSJ south entrance.
- 4) Ensure that both the north and south headhouses strive for the highest level of iconic, context sensitive design, including art and design cues from the surrounding Downtown community.

Diridon BART Station

- 1) Locate BOH underground.
- 2) If all options in attempt to locate the BOH underground have been exhausted, then:
 - a) Provide an in-depth presentation to the Oversight Committee and Diridon & Downtown Community Working Group on the reasons why; including a comparison with Downtown San José Station and its BOH location; and
 - b) Line the edges of the BOH with active, ground-floor space such as retail along its sides facing Cahill St and Montgomery St; if this cannot be accommodated, provide an alternative plan for how the above-ground BOH facility will be activated at street-level and vet this with City, Oversight Committee, and Community Working Group.
- 3) Redesign the tunnel to facilitate a future passenger connection to the Diridon Intermodal Facility; specifically, work with the Diridon Integrated Station team to identify a location for a knock-out panel and include the knock-out panel and mezzanine extension the BSV project design.

STATION DESIGN AND ENGAGEMENT UPDATE: VTA'S BART SILICON VALLEY PHASE II EXTENSION PROJECT



VTA's BART Silicon Valley Phase II Extension Project (VTA's BSVII) is a six-mile, four station extension of Bay Area Rapid Transit service from the Berryessa/North San José Station through downtown San José and the City of Santa Clara. This final extension will achieve the vision set by residents of Santa Clara County over 20 years ago and deliver the final missing segment for ringing the Bay with high-frequency, high-capacity rail transit.

In May 2024, VTA submitted for entry into the Federal Transit Administration (FTA) New Starts Engineering Phase. This is a major milestone on the path receiving of a Full Funding Grant Agreement (FFGA) from the FTA, which is targeted by the end of 2024.

Recent stakeholder and community concerns have been raised regarding the current design of VTA's BSVII facilities, with particular focus on the three underground stations within the City of San José. This document highlights VTA's approach for addressing the feedback and concerns that have been raised regarding design of the 28th Street/Little Portugal Station and Diridon Station, as well as the desire for an additional entrance to be provided at the Downtown San José Station.

28th Street / Little Portugal Station

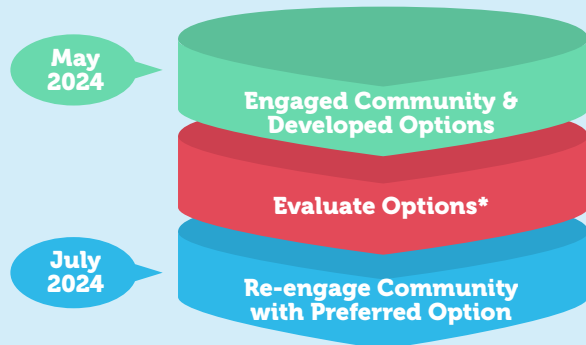
Background

The 28th Street/Little Portugal Station will be located at the northeast corner of N 28th Street and Five Wounds Lane. The latest designs include a station entrance building fronted by a station plaza. Additionally, station infrastructure facilities (previously referred to as Back-of-House facilities) are at-grade, located behind the station entrance building. There are concurrent planning efforts for the station site. VTA's Design Development Framework (DDF) will serve as the vision for development of the VTA's land around the station (~13 acres). The City of San José's Five Wounds Urban Village Plan Update is a land use and circulation long-range plan for the communities around the station.

Stakeholder and Community Concerns

Concerns	Approach for Addressing Concern
<ul style="list-style-type: none"> Perceived reduction in size of the station plaza 	<ul style="list-style-type: none"> Project able to maintain size of plaza previously envisioned
<ul style="list-style-type: none"> Concerns that the at-grade station support facility building will not provide ground-floor activation 	<ul style="list-style-type: none"> Project is exploring configurations that include undergrounding part or all of the station infrastructure facilities to reduce overall footprint at-grade Project working with DDF Team for allowing development on one side of station infrastructure facilities if they remain at-grade
<ul style="list-style-type: none"> Perceived impact on total on-site development due to at-grade station support facility building 	<ul style="list-style-type: none"> Project working with DDF Team on maintaining development potential of site

Timeline for Addressing Stakeholder & Community Concerns



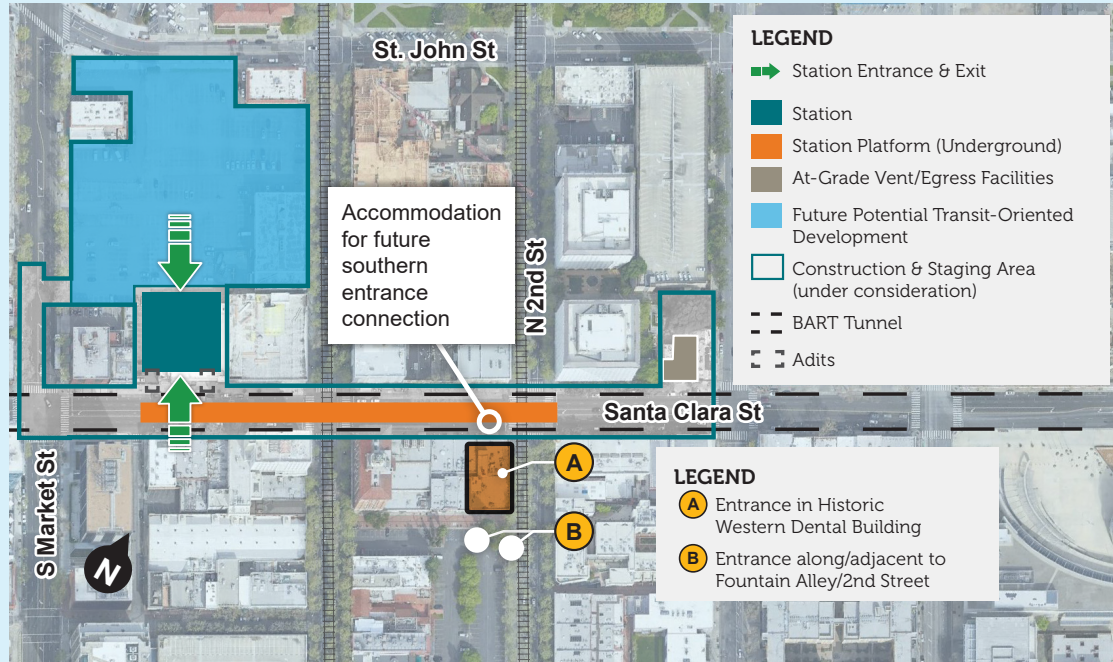
* **Evaluation criteria includes:** construction and O&M cost impacts, schedule impacts, passenger experience, effects on utilities, and Stakeholder & Community feedback.



Conceptual rendering of 28th Street/Little Portugal Station Plaza

Background

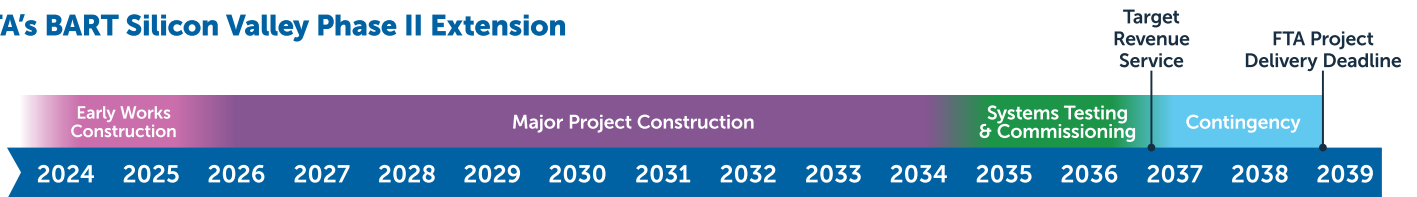
VTA and the City of San José have worked closely to discuss the feasibility of a south entrance to the Downtown San José Station through multiple coordination efforts. In 2022 the tunnel’s configuration was updated from a stacked platform configuration to a center-platform configuration, enabling the possibility for direct access to the station concourse and platform from the south of the tunnel – which was previously deemed infeasible. Given the lack of environmental clearance for a south entrance, and not wanting to delay the process for advancing VTA’s BSVII Project, VTA refined the design of the tunnel to enable a future connection shown on the site plan, below. The site plan also shows locations that VTA and the City initially identified for evaluation. To advance, concepts at these locations must address previously documented issues and avoid adverse effects to historic/cultural resources.



Schedule for Delivering the South Entrance by Opening Day

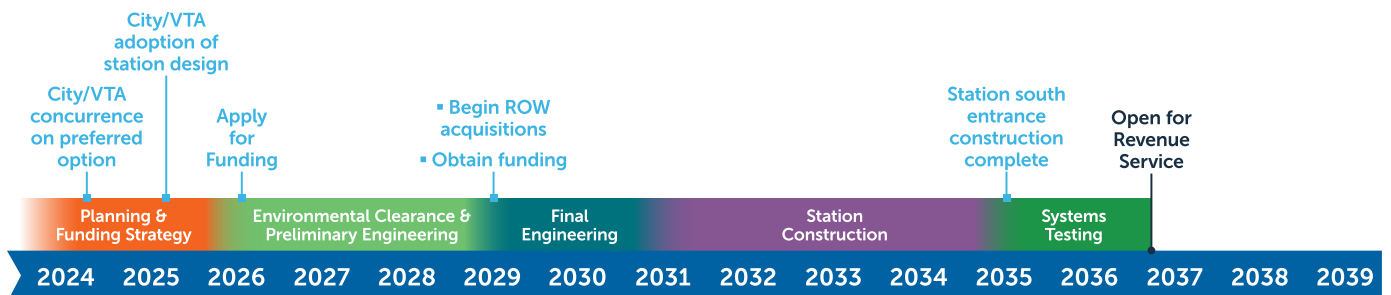
VTA is committed to delivering a south entrance for the Downtown San José Station in time for opening of the extension, which is targeting Revenue Service in 2037. The timelines below depict how the two projects will be aligned.

VTA’s BART Silicon Valley Phase II Extension



Based on preliminary schedule for planning purposes.

Downtown San José Station - South Entrance



Based on preliminary schedule for planning purposes.

Approach for Advancing Planning & Funding Strategy

VTA and the City of San José will partner and engage key institutional stakeholders, such as San José State University, to collaboratively advance key decisions required for initiating pursuit of the Downtown San José Station South Entrance. A preliminary workplan for advancing through the Planning and Funding Strategy Phase (*shown in orange in the timeline on the previous page*).

June–July 2024

- Develop an approach for engaging with FTA
- VTA-City of San José to develop an approach for site selection, environmental clearance, funding, design, and construction for south entrance
- Establish VTA-City of San José South Entrance Task Force to:
 - Establish roles and responsibilities
 - Review and confirm workplans
 - Identify & evaluate feasible south entrance options
 - Develop a selection process for the preferred option

August 2024

- Task Force to:
 - Evaluate feasibility criteria
 - Identify options for concept development
 - Identify preferred environmental review process
 - Identify potential project delivery methods
 - Explore funding options for delivery of south entrance outside of BSVII funding

September–October 2024

- VTA to complete concept design for one or more south station entrance options

October–November 2024

- Task Force to:
 - Select preferred option
 - Identify funding source(s) & establish funding strategy for environmental clearance of preferred option
- City & VTA Board concurrence on preferred option

December 2024–Following Receipt of FFGA for BSVII

- VTA to initiate CEQA/NEPA environmental process
- VTA to initiate any third-party agreements necessary for advancement of preferred option

Potential Funding Sources

STATE

FEDERAL

The South Entrance Task Force will develop a funding strategy for all phases from pre-construction activities (planning, environmental clearance, and engineering) through construction and project delivery. A non-exhaustive list of potential funding sources that will be further evaluated and pursued is provided below.

- **Caltrans:** Sustainable Transportation planning Grants – Next cycle 2025
- **California Air Resources Board (CARB):** Sustainable Transportation Equity Project (STEP) – Next cycle Fall 2024
- **Caltrans:** Low Carbon Transit Operations Program (LCTOP) – Next cycle 2025
- **California Transportation Commission (CTC):** Local Partnership Program (LPP) – Application due Fall 2024
- **California Transportation Commission (CTC):** State Transportation Improvement Program (STIP) – Next cycle 2026

- **US DOT:** Rebuilding American Infrastructure with Sustainability & Equity (RAISE) Discretionary Grants – Feb 2025
- **FTA:** Capital Investment Grants (CIG) – Section 5309
- **US DOT:** Safe Streets and Roads for All (SS4A) Discretionary Grant Program – September 2024

Background

The Diridon BART Station will be between Montgomery and Cahill Streets, just east of the existing San José Diridon Caltrain Station. The Station is being designed with at-grade station entrance building with below-ground station concourse and platform. A building housing station infrastructure facilities is also planned immediately south of the entrance building.

The existing San José Diridon Caltrain Station is a historic station that is currently serving Caltrain, Capitol Corridor, Amtrak, and Altamont Corridor Express (ACE) regional train services. VTA also has a transit center and a light rail station serving this station.

VTA, JPB/Caltrain, MTC, the City of San José, and the California High Speed Rail Authority (CHSRA) have partnered to develop a vision for a future Diridon Intermodal Station that will accommodate the station's future capacity needs. The future Diridon Intermodal Station is being advanced separately from VTA's BSVII with ongoing coordination and collaboration to ensure alignment for delivering a future cohesive station area.

These stations will be at the center of adjacent development, which will transform land surrounding Diridon Station into a dense urban environment comprising multi-use developments that will activate the area.

City of San José Concerns

The City of San José is concerned with the current design of Diridon BART Station and its facilities, including:

- The station is 'too far' from the future Diridon Intermodal Station. Once both are constructed, the Diridon BART Station should be seen as an integrated component of the future Diridon Intermodal Station.
- The above-grade station infrastructure facilities impact the ability to activate ground-floor space around the station.
- The above-grade station infrastructure facilities limit the potential for development and/or the creation of an active public space within the planned dense urban environment.

VTA and BART Considerations

The Diridon BART Station is being designed with key considerations by BART, who will operate the station, and VTA, who will own and pay for all costs associated with the station's construction and operations:

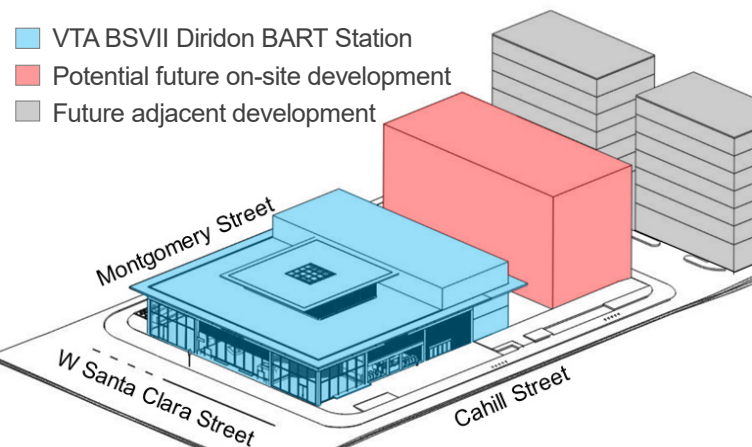
- BART desires station infrastructure facilities to be provided at-grade to facilitate maintenance access.
- VTA aims to reduce both construction and O&M costs by providing station infrastructure facilities at grade.
- The future Diridon Intermodal Station has many unknowns (e.g., alignment, concourse footprint, schedule, and environmental clearance) that limit VTA's BSVII ability to design for future accommodations.

Addressing Stakeholder Concerns

A Diridon Task Force comprising VTA, JPB/Caltrain, the City of San José, and CHSRA has been established to evaluate potential for:

- Direct underground connection to the future Diridon Intermodal Station.
- Relocation of the Diridon BART Station's west vent shaft.
- Configuration of necessary station infrastructure facilities
- Conflicts with the future City of San José Airport Connector.

Rendering of concept for evaluation by Diridon Task Force



Project Funding Sources

